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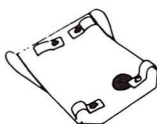
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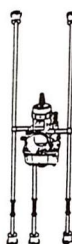
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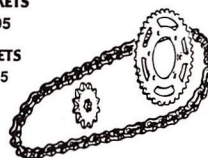
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On the cover: Tommy Norton had to do or die at the final NETRA hare scrambles at Lembo Lake, and when Scott Phelps dropped out all the pressure was off. He still rode like a wild-man in the mud, though.

March 1996
Volume 26 Number 3

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Warning: Anyone harboring conspiracy theories around the content of this magazine is sadly deluded and should immediately visit a physician for a Prozac perscription. Trail Rider is a hand-to-mouth operation, and we are not capable of anything loftier than putting out a magazine each month as sloppily as we can. We recommend that you ride carefully, dress in all the protective gear you can hold, and know in advance that off-road riding can be very hazardous if you don't keep your wits about you. Remember that this whole sport is based on fun, and if you're not having fun you're going about it seriously wrong, and should find something else that makes you happier. Life is too short, eh?

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LAST OVER

by Paul Clipper

To Screw, or Not To Screw

There was a time when finding a screw in your tire meant you were sitting alongside the trail with either a long walk or long push ahead of you...or, if you happen to be one of those people who really do carry tire changing tools with you, fixing a flat. Flat tires have been responsible for plenty of interrupted and aborted rides, and I can't count the number of unintentionally picked-up sheet metal screws and nails I've seen. However, there is a time of year when having screws in your tires is a distinct advantage.

Just this past weekend, as a matter of fact, we gathered once again for the annual New Years Ride. As it turned out, this past January first was a "classic" weather day—just about freezing in the morning, with a complicated layer of white stuff on the ground. I say complicated, because in New Jersey we never get just snow. For reasons probably related to proximity to the ocean and frequency of toxic waste dumps, every winter storm we have down here elaborately covers the gamut of possible weather conditions. The storm that covered us for January first, for example, started out as rain on frozen earth. When the ground was sufficiently puddled, the rain changed to large hard balls of sleet. Once that was bedded down solid and starting to turn white it changed back to rain, then to sleet, then to rain again; and finally I think we actually saw a little bit of real snow. And then it got cold; like in the 'teens.

The net effect of all this weather abuse was to wrap the countryside in a cast-iron-hard shell that was completely impenetrable by normal tools. You could use a pickaxe on a sidewalk, and not do more than chip it; if you managed to get down to the concrete there was no way the ice would release. Not good gardening weather, no doubt.

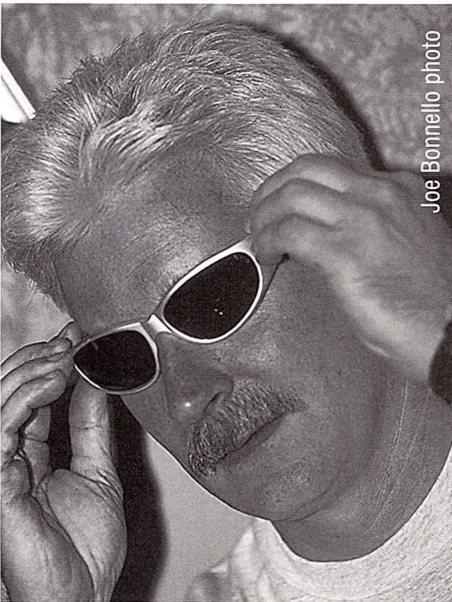
It's also fairly poor weather for riding, unless you're really into it. If you're a true believer in winter riding, then you know that the conditions described above are actually perfect, and leave a real snow rider nothing to complain about at all. However, I'm talking about people in the near southern latitudes here, and generally South Jersey residents do not look forward to riding in the snow. By and large, we like it a little more balmy than that.

However, there was a New Years Ride to run, and tradition carries much more weight than weather. We'd learned from the hard winter of '93-'94 that if you're going to actually ride in the frozen wasteland, you have to make some sort of concession to traction. Brand new tires are, at the very

least, a good idea. Big, sharp-knobbed mothers with plenty of space between the knobs. If you go for new tires before a good snow ride you are assured of two things: you may not fall down in every corner, and you could possibly see the better equipped riders whenever they stop.

On the other extreme is a set of dedicated carbide-spiked ice and snow tires, such as the ones made by Trelleborg or the spiked Cheng Shins offered by Motorace. Every real enthusiast in New England has at least one set of these things sitting in their garage or barn, and they dutifully put them on in the late fall and only reluctantly take them off when the daffodils are blooming.

Spiked tires are the absolute ultimate, but they come with a few built-in problems. First, they appear to be really costly. They're initially expensive, but with reasonable care you might be able to get three seasons of use out of them. Second, they're very temperature-sensitive (the rub-



Joe Bonnell photo

ber's so soft that they wear out rapidly when the temps get above freezing), and can be damaged easily by riding on bare ground. Down here, since one weekend it can be 50 degrees, while the next weekend it might be below zero, you'd have to be committed to changing tires nearly every time you ride. Very few people really like to change tires, according to what I've seen, and once they change a tire with 300 sharp carbide studs sticking out of it they're going to like it even less (don't wear anything you don't want perforated).

So the key to traction down here is driving sheet metal screws into your knobs. It's cheaper than a set of dedicated tires, and if the weather changes, who cares? You're not going to wreck a good set of expensive tires. Most southern riders I know prepare for snow riding by going through their used tire stash and hunting out the ones that are only half worn, and have big broad knobs to play with.

Then, they get some screws. There are two ways to go here. You can either buy the

dedicated Gold Max or Kold Kutter ice screws that you see advertised here and there, or you can go to the hardware store and take pot luck. Everybody has a different theory about screws—everybody uses hex-head screws, but what size to use is a point of constant debate. Some use #8, some #10, and Mark here swears by #12 sheet metal screws; and it seems you can get away with half-inch or five-eighths length, depending on how worn the tires you're studding happen to be.

Then, you sit down with an electric drill with the appropriate hex driver bit, and proceed to drive the screws into the knobs. It takes a lot of screws and a long, boring time. Have you ever counted how many knobs there are on the average tire? You want at least one screw in each, and maybe two in any of the broad knobs. A lot of guys get tired and start skipping side knobs when the trigger-finger blister starts forming; these are the guys you see hitting the ground a lot. There also is a lot of discussion in the really lazy camp about if you need to stud the front tire at all. Here's our simple formula for dealing with the issue: if you want to go forward, stud the back; and if you want to be able to steer, stud the front.

Once your bike is completely screwed up (so to speak), you can take to the snow covered trails with complete disdain. You'd get just a little better performance overall with real spiked tires, but yes, the screws work quite well, and contrary to uninformed opinion they don't come raining out like shrapnel at the guy behind you once you start gassing it. You may lose a few on a long day, but they stay put pretty well. Well, as long as you're just riding in snow, and as long as you don't spin it too hard going over logs. But, it's either that or don't ride, right?

The New Years Ride this year was positive proof that screwing was the way to go. Oh, one or two of the guys actually had real spiked tires mounted up, but the majority used screws. How much time they spent—in other words, how many screws they installed—seemed to determine how far up in the pack they rode. If they started out with a box of a thousand screws and didn't get tired or cheap after a few hours, they stayed right up front with the leaders and had a ball. If they started skipping knobs with the screw gun, well, riding ability aside, they moved back in the pack. Far in the back we found the guys who left preparation to chance, and either ran unscrewed new knobs or the same old worn knobbies they finished the warm season with. One unfortunate soul was spotted riding the course with plain OEM dual sport tires on his machine. Well, he wasn't so much riding as dog paddling.

So if you want to ride all year, now you know what you have to do. You either screw up your tires or screw up every muscle in your body from crashing in every corner. And when you find a sheet metal screw in your freshly-flattened tire this spring you won't wonder where it came from any more. It was from all the fun you had last winter...or maybe all the fun everybody else had last winter! □

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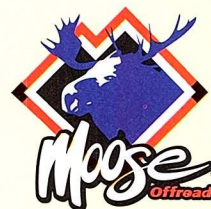


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MAIL ENTRY

Adventures of a Horribly Scarred Young Boy, Act II

Dear Trail Rider,

Just finished reading Charlie's article in the January TR. Where in the hell did you find this guy?

Every time I've read one of his stories, I laugh until I choke. My wife comes over to see what's so damn funny, reads a couple lines, and walks away shaking her head mumbling something like "...I still can't understand why I married you..."

I actually had the dubious pleasure of meeting this twisted individual last spring. I was working Check 5 at the Little Burr National Qualifier in MacArthur, OH. It was quite rainy, and well...you know southern Ohio. The riders coming into the check looked barely human. I pitifully looked upon one slightly overweight, balding fellow who was attempting to irrigate his red and swollen eyes with the hose from his Camelback. He inadvertently rinsed the mud off of the area of his chest protector which proudly displayed a "Team Mooch" sticker. I walked over to this whimpering creature and asked "You wouldn't happen to be that guy who writes funny articles for Trail Rider, would you?"

His demeanor changed instantly from pure misery to an amusing combination of paranoia and shock. I think he was initially flabbergasted that someone would actually recognize him, and in the next instant, he acted like I was an IRS agent asking for an audit.

"Uhhh, yeah....how'd you know?"

"I saw the "Team Mooch" sticker and took a wild guess."

"Oh, yeah....OK. You wouldn't happen to have a clean rag, would you?"

And that was about the extent of it. It was day two of the qualifier, and at this point, few of the riders were capable of carrying on a sensible conversation. When he was finished ruining my handkerchief, he did mumble something about losing his fanny pack on the course. He said his camera was in it, with the pictures he was supposed to use for an article.

I guess he never found it. I saw his article, but didn't see any pictures from the event. All I remember is something about a Rapala fishing lure strung between his nipples. Wow. You DEFINITELY don't pay that guy enough.

Scott Whitford
via e-mail

We pay him plenty, Scott, and luckily he lives in a Slovakian neighborhood and can actually spend all those Czechoslovakian kroners we brought back from the '82 ISDE. When we run out of kroners we're going to start paying him in grain and gov-

ernment-surplus cheese. He's an ex-hippie, he knows what to do with that stuff.

Actually, we met Charlie at the Tulsa Six Days, where he was an actual bronze-medal finisher of the event. It all started when he was walking by with a horribly bruised right thigh and Bernardo ran up, grabbed him and licked his leg (true story). Charlie was stunned by all the attention, and started jabbering about how he can write "real funny" and could be a great addition to Trail Rider. Yeah, like I need another Hoosier hanging around. I didn't want to write the whole ISDE story myself, so I said "Sure, write something about the Six Days." He looked at me real serious and said, "Yeah...but there wasn't anything funny about this week...." One thing led to another, and now he's all ours.

Oh, and by the way, he really did lose his camera at that event, so if any honest Qualifier rider out there found it, please get in touch with us. Until he gets another camera he's going to continue drawings pictures, and well, you know....

What Kind of a Guru Are You?

Trail Rider,

Dear white haired guru of the east; I have discovered a problem with my '96 KTM 250 E/XC. When the forks bottom out, the odometer cable breaks! Needless to say I was bummed yesterday when I went out to get the mileage of a course we are setting up, only to find my odometer was not working. We were jumping off of a neat old growth stump while warming up the bikes, and my odometer was stuck at 7 tenths of a mile. I looked at the cable and found it snapped right above the guide which mounts to the fork. I looked at some pictures and it seems that all of the other companies gave their cable some room to move. It looks like I'll be getting a Husqvarna cable guide for my forks. Hopefully KTM will warranty this problem and come up with a fix soon, so that everybody with an E/XC doesn't have the problem.

See Ya,
Spike (Mike Sheetz)
via e-mail

Yo, Spike. When you set up the front end on the Marzocchi-equipped KTMs you want to make sure you don't have the odometer cable guide mounted too high on the fork. Last year we saw the problem before the cable broke, and since the stock guide can't really be repositioned effectively we instead zip-tied the cable much lower on the fork. It saved the cable, and so far even our '96 hasn't broken (although it has cracked, and been wrapped with tape).

Lafferty Fan

Dear Trail Rider,

Hey! I have a great idea for next year ('96). Why don't you pack your magazine full of articles and pictures of the Lafferty boys every month. Zzzzzzz! Been there, done that!

New ideas:

Women enduro riders.

Publish riding areas—New England,

Allegheny National Forest.

Cycle trailer shootout—capacities, weights, 3-bike, 4-bike.

C rider highlights.

Bisonhead Racing,
Manheim, PA

Cycle trailer shootout? Wake me when it's over....

Trouble with the Laffertys is that there are so many of them, and they're so much better than you or I they can't help but get their name in the paper very often. We're going to try to widen our focus a little in '96, though; and like the idea about a woman enduro rider article. How about female hare scrambles riders? There's a pack of them in NETRA. As far as riding areas go, as soon as we get some that everyone will admit are truly legal, we'll tell you all about them. And we'll do the trailer shootout right after the tie-down shootout.

Get to Love Our Advertisers

Dear Trail Rider,

I've been a non-stop subscriber to Trail Rider and the New England Trail Rider when Bob Hicks put it together in his office above the barn.

I had to write you about your Nevada Rally article. An excellent job on the article, and mainly on finishing the event! However when you mentioned there wasn't enough pages/space available in the magazine, I was floored. Every issue I go through and pull out all the mail order mini catalog inserts, etc., I end up with a magazine the thickness of a pamphlet or newsletter. I know you need the advertising, but for me I need more, and longer, articles like the rally.

Daryl Dowling
Water Mill, NY

Thanks Daryl, the rally was an incredible experience, and there really wasn't enough space to do it justice, at least not enough to use without annoying the people who really don't care about what happens in Nevada. The advertising is a tough thing. We would absolutely love to do a magazine with no advertising at all—except maybe Yankee Trader ads—but there is simply no way. I just ran a quick report out of the accounting computer, and the last issue we printed cost us at least \$2.20 per magazine. Your subscription pays \$1.50 per magazine, and since we're only selling to about 3,000 people every month, we need a high percentage of advertising pages in order to make a living (yep, this is all we do here). We could possibly do a book with no ads, but a subscription would have to cost you around \$50 to make it worthwhile, and not even Consumer Reports asks that much.

And even so, most of the advertisers in Trail Rider are here because they want to do business with you, being that you're an easterner. We get lots of interest in advertising space simply because we're not a west coast magazine, and maybe there's some sort of a proud distinction in that. But I know what you're saying, and I sympathize. □

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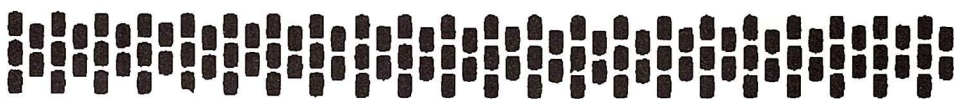


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EASTERN NEWS



Help the Menzes

Before we came along, Trail Rider was owned and run by John Menze and Gail Pells, a pair of riding enthusiasts from Connecticut. Along with raising their two children, Kyle and Ross, the Menzes put out Trail Rider for a couple of years, after buying the magazine from Bob Hicks. John went on to riding and racing mountain bikes, and then got into racing BMX with son Ross. Things were going along well, until Kyle was diagnosed as having T-Cell Lymphoma nearly two years ago. Since then their life has been one round of chemotherapy after another, and a mounting pile of insurance co-payment and out of pocket drug bills. Daughter Kyle is doing well, and is optimistic that this terrible treatment she is being subjected to will save her life, and the whole family is behind her, and helping her get well.

The debt load this has forced on the Menzes has been astronomical, and to help out a fund has been established in Kyle's name. If you're a long-time reader of Trail Rider you know John and Gail, and we urge you to give what you can and help them out. Any donations can be sent to The Kyle Menze Fund, Center Bank, 260 Amity Road, Woodbridge CT 06525.

Hertfelder is Here!

By now it should be old news that Ed Hertfelder's Duct Tapes column has been removed from its monthly place of prominence at Dirt Rider magazine. We knew

Rousing Success

In the back of this issue you'll find an article about the Meteor M.C. ECEA hare scrambles from last year. The interesting thing about this event was that it was staged on a piece of ground technically owned by the New Jersey Conservation Foundation. For want of a better phrase, we can safely refer to the NJCF as "tree huggers." The way Meteor gained use of this property is through some quick maneuvering on the part of the New Jersey Trails Conservancy, a group established nearly two years ago to fight for riding rights for New Jersey off-road motorcyclists and ATV riders.

Three years ago, the NJCF came to Lindsay Pirie, whom they knew from his work with the state Trails Council (as a representative for "motorized" trail use), and asked if he knew of an organization of motorcyclists in the state who would like to assume stewardship of a piece of land we know as the Chatsworth Triangle. The Triangle had fallen into hard times, and become a late night party spot, and favorite place for local stolen car burners, and it was frankly an embarrassment to the NJCF. They wanted a group of off-roaders to "take it over," and had determined that its potential as anything other than an off-road riding spot was about zero. In other words, they wanted nothing to do with it and were ready to admit the Triangle's complete lack of value to the Sierra Clubber set.

Trouble was, the ECEA, which was the strongest motorcycling organization we had at the time, didn't want anything to do with it, for a variety of reasons best not to go into. So nothing happened at the time, but after creating the New Jersey Trails Conservancy, we figured "why not?" and went back to the NJCF and made them an offer. They were anxious to agree, and now the NJTC (don't get the acronyms mixed up) is more or less running the property, where part of the rules set down allow the NJTC to run one orga-

nized event per month, as well as access to card-carrying NJTC members for "maintenance and policing." The Meteor hare scrambles was just the first in what could potentially be a monthly thing.

Now to the point. Paul Clipper was the founding president of the NJTC, and last year Lindsay Pirie headed up the organization, as well as continued his work with the NJ Trails Council. Lindsay did his time, and now the NJTC needs a new president. Patti Hodges is currently the paid director of the NJTC (okay, frankly the person who does all the work), and Patti now has a new job and wants to pass on her NJTC responsibilities. Aside from these two official positions, the NJTC basically wants support. If you're interested in the future of riding in New Jersey, be it on motorcycles, ATVs, or minibikes with your kids, please get in touch with the NJTC and see what you can do to help.

If you're wondering what's in it for you, know in advance that the NJCF so far is tickled pink with what we've done with the Triangle, and they are holding title on many other little pieces of land that could also easily be used for off-road recreation. All it takes is a little bit of work from interested enthusiasts like yourself, and we could have plenty of really fine legal riding in New Jersey. You can contact the NJTC at (609)657-6338, or get in touch with us here at the magazine and we'll help you get hooked up with them.



DMS Rider Marc Grossman

**Honda Kawasaki
Yamaha**

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March 1996

3/17 NETRA Annual Meeting, Oxford MA
3/17 Sandy Lane Enduro, Greenbank, NJ
3/31 Greenbrier Enduro, Belleplain NJ

about it a while back, when Ed called to tell us he was getting the axe. Something about "not fitting in with Dirt Rider's new editorial direction." Well, the last thing we would want here at Trail Rider is to see an end to Ed's musing on motorcycle sport, so the two of us got together and came to an agreement. As it stands right now, Ed is going to be doing a story for us every other month, and maybe we'll turn it into a monthly thing when we can afford it. Some of his stories are going to be classics, like this month's submission, The Prodigy, on page 46, and we're also going to try to get him to write about one or two of the many dual sport rides he attends every year. We couldn't let him go completely; as it is we're letting him go from the local scene, since Ed and his lovely wife Bernice moved to a tar paper shack in Cabot, Arkansas, this past January. If you want to get in touch with him, his new address is 111 North Summit Drive, RR5, Cabot AR 72023.

Awards Everywhere

We are packed to the rafters now with lists of names, awards winners, statistics, and all the things that occupy your time in the off-season. In short, yes, it's banquet season again! You can read about the GNCC banquet in this issue, and next month we will have coverage on the NETRA banquet and the ECEA banquet. The NETRA banquet happened last weekend, and it was a rousing success, with 390 dinner tickets sold and more people than that attending, easily. Everybody had a ball, and the sponsoring CATRA club did a bang-up job. And, the Sheraton Saratoga Springs was a fine place. This weekend's the ECEA banquet, at a new location in Trevoze, PA, and we're all anxious to see what it'll be like. Stay tuned, we'll have the details next month. □

NAMES AND ADDRESSES

New England Trail Rider Association (NETRA)

P.O. Box 478
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THE REST of the WORLD

New Trail Bike?

What would you think of a new inexpensive off-road motorcycle made on the east coast of the USA? We received a faxed press release from a company called Megabike North America, saying that they were planning on releasing information on a new American bike called the Traxx JH125L. With a suggested list price of \$1995, the JH125L is a four-stroke 125cc machine with "Showa forks, a CDI ignition, headlight, ignition lock and more." It sounds like a humble beginning, for sure, but don't sell them short. We will be able to see the bike in Cincinnati in the middle of February, so hopefully we can have a photo of it in next month's issue. In the mean time, if you want more information it can be had from Megabike N.A. Inc., 12640 Old US Hwy 52, Winston-Salem NC 27107; (910)764-9828.

TM Off-Road Team

TM Motorcycles recently announced its national off road team for '96. The team will be based out of Town & Country Cycle Center in Sussex, NJ, and will consist of six riders—Chris Smith, Drew Smith, Kevin Bennett, Jeff Kirchner, Scott Chapkovich and Eric Koeller. This six man squad will contest the AMA National Reliability Enduro Series, the AMA National Enduro Series, and selected GNCC events, all on TM motorcycles. Main sponsors for the team include Works Enduro Rider, Arai Helmets, JT Racing, Smith Goggles, Splitfire Spark Plugs and Sprocket Specialists.

Spring Tour

There is still time to sign up for the Trail Rider Spring Nevada Tour, being organized by Nevada Motorcycle Adventures. We call it the Spring Tour, because one of the staff of Trail rider is pledged to ride it, and it's also in the spring—

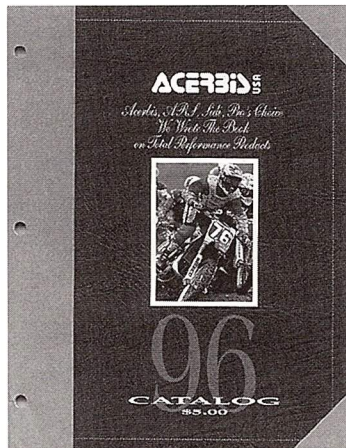
April 28 to May 3, to be exact. The tour will cover dual sport trails from the California border to central Nevada, and will cost \$1100, which includes a motorcycle, guides, support truck, lunch, and lodging each day. It should be fun and easy trails. For information call (702)359-4380.

New Cheap Huskys

Has anyone noticed that Husqvarna seems to have slashed its prices on the '96 model line? If you've ever thought about owning a Husky, apparently now is the time, since the 1996 machines have the new Marzocchi forks and seem to be selling for around \$500 less than everything else out there. See your Husky dealer for the details, but we're sure you'll be surprised and pleased.

Catalog O'The Month

This month the new Acerbis catalog came tumbling in the front door. It's chock full of everything they sell down there at the great purveyor of plastics business,



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and it only costs \$5. Costs? Yep, but it comes with a \$10 gift certificate, so you actually make money by buying it! Send your half a sawbuck to Acerbis USA, 9402 Wheatlands Court, Suite A, Santee CA 92071

Dakar Rally #4

Since Cycle World staffer Jimmy Lewis was competing in this years Granada/Dakar rally (sometimes known as Paris/Dakar, depending on where it starts), we were hooked into the fax communications coming from Africa on the event. Jimmy wound up dropping out about halfway through the 15-day event with severe dehydration and general body problems, and the lead in the event swapped around to keep things really interesting. IN the beginning it looked like Stephane Peterhansel was going to rule the whole trip, as he's done four times now, but it wasn't to be so. On the sixth day, Peterhansel had fuel problems that eventually forced him out of the event, while at the same time Heinz Kinigadner wrecked his engine, after working his way up to a close second place. This cleared the way for our old Nevada Rally buddy Edi Orioli to take over the lead, which he never let go of until the very end. This win ties Orioli with Peterhansel's four overall wins at Dakar. What was it really like? "Frightening!" according to Lewis. "The Dakar Rally makes the Nevada Rally look like a ride around the block."

MotoWorld Moves

If you're looking for ESPN's MotoWorld in all the usual places, you might not find it.

That's because the MotoWorld show has been swapped to the ESPN2 network, where it will co-exist with Jerry Bernardo's MotoWorld 2 show. Why have them both on the same network? Because ESPN is changing its format to be all stick and ball sports (boring...), and ESPN2 is now going to be all the motorized and exciting ragged edge stuff. Why don't you get ESPN2 even now, through your cable company? Beats us; the best thing to do is call your cable carrier and tell them you want ESPN2, as soon as possible. Us? We don't even have a TV out here in the woods, so what do we care.

Vegas to Reno

They're calling it the "Longest Off-Road Race in the U.S." and there's no reason to believe it's not true. This month on March 29 Casey Folks will flag off the first riders in

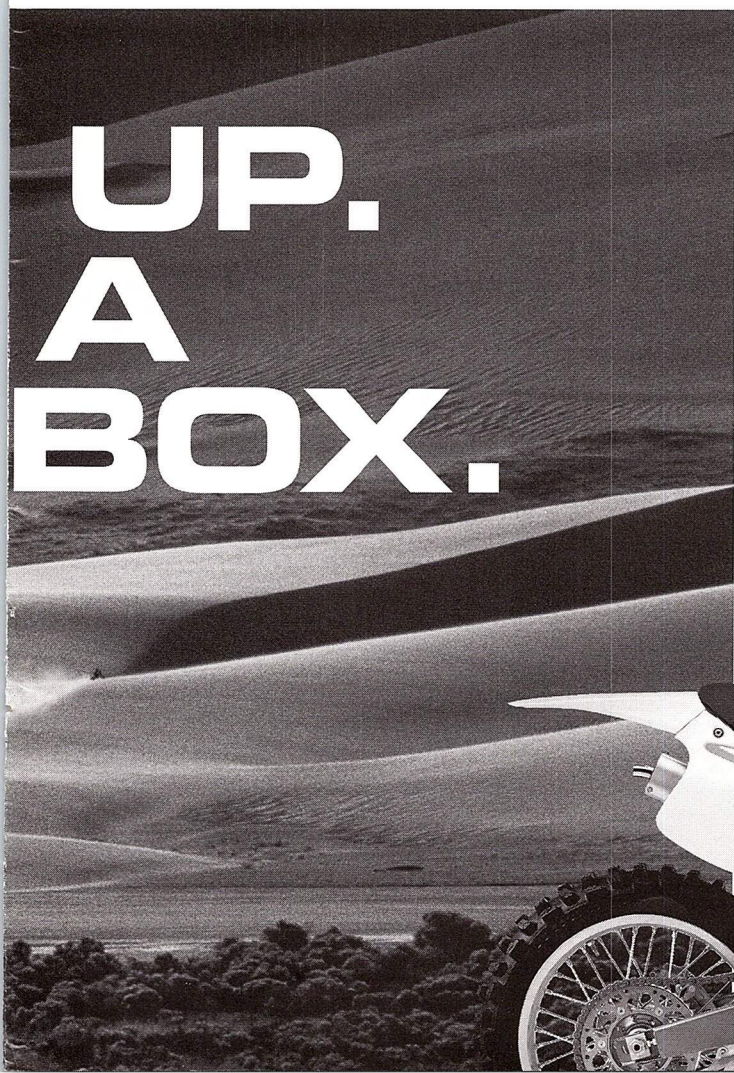
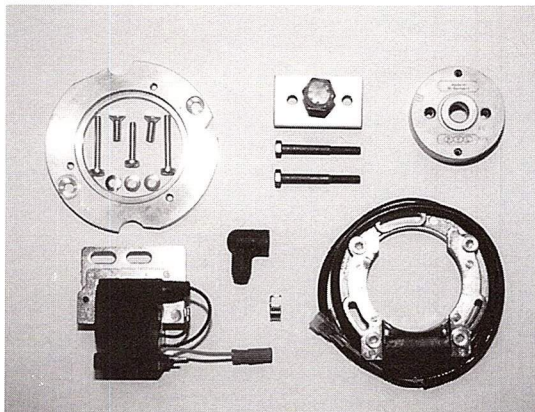
the inaugural Vegas to Reno event, and it's going to be a really cool race. Both trucks, cars and motorcycles and quads will be racing the event, guaranteeing plenty of exposure and hopefully a long-lived career. Will we be there? Who knows! It isn't even February as this is being written, and we have to get to Cincinnati yet! We'll let you know what happens, whether we see it live or not.

Mystery Under Investigation

After allegations made by world traveler Sidney Dickson that he may actually be Ed Hertfelder's son, Trail Rider has begun an investigation to clear up the matter once and for all. We have sent a sample of Sid Dickson's blood to a DNA lab. As Hertfelder had no blood to spare, we are waiting for a sample of his hair. This may take a while. We'll keep you posted.

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BLACK & BLUE

Hines takes the final, and another year ends

Stafford, CT 11/5/95

There really wasn't much in the way of surprises possible at the final NETRA enduro. Kevin Hines had already locked up the season championship, with a perfect set of wins for every event he entered. Surprised? Not really.... Neither was there a battle going on for second, since Mont

Fairfax had strong enough at each event of the season to take a solid points lead over Paul Milliken, in third place. Mont even won the overall at the only event Hines missed, the King Philip West. He went into this last event with about 97 points to Milliken's 69, so there really wasn't much Paul or anyone else could do. Hines, on the other hand, had 180 points going into the Black & Blue, so he was really out of the ball park.

Still, it actually turned out to be a good day for the B&B, with mild temperatures (for November) and a welcome lack of rain, snow, ice or other forms of natural punishment. That's not to say it wasn't wet or muddy in spots. There were a few places where the water was running freely, but it was up to you whether you stayed dry or not.

In spite of his extreme lead, Hines left the start just as determined as if the Black & Blue were a national. He was charging hard in one of the early sections, and managed to crash hard enough to tear all three of his timekeeping computers right off his handlebars. He dropped his mangled timekeeping gear off at the gas stop, and rode the rest of the run with a wristwatch and no odometer. "What do I remember about the run? I remember my hands were cold," he told us afterwards. The lack of timekeeping gear didn't do a whole lot to slow him down, and he finished the event with a 16, four points better than second place. Next year he claims he's going to ride all the nationals again, which should ease the pressure on the rest of the NETRA riders considerably!

The run was normal this year, and if you're familiar with typical northern Connecticut woods riding you know we mean there were a few rocks out there. There were rocks that would have made Dave Coombs proud, as Charlie would say. But separating the rocks is just enough nearly-perfect trail to keep you going; keep you enthused.

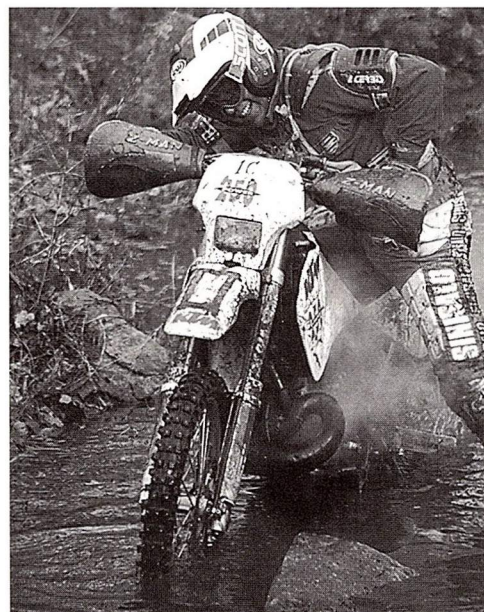
We watched one such nifty spot early in the day. The riders came out of the woods into a fairly open clearing on the side of a hill, skirted the side of the hill going down, then made a turn to the right and crossed the trough between that hill and the one on our side. Except for the rocky waterhole in the trough, it all looked wide open...except for the roots and logs under the fallen leaves. We watched the first rider come hammering down the trail into the opening, and negotiate a little right-left jog



Kevin Hines finished up the season with another win and a perfect score for the year—an overall win for every time he rode. Most will be thankful that he's riding the Nationals in '96.



The Black and Blue lived up to its name, with plenty of opportunity to beat yourself that color on the rocks. It was a workout!



Al Zitta does the "stretch-for-life" after bobbling in a waterhole early in the run. He might be a little out of shape, but he's still grinning—and still winning!

before the right turn. Everything was going well, and we all had camera raised for the waterhole, when he hit the jog and went down like a sack of turnips. Can you say "root?"

The waterhole in this spot wasn't that long or deep, but there wasn't any way around it



Remember when you had guts like this? Josh McLevy leaps a waterhole without hesitation, and everybody else paddled through. Watch out for him this year.



'95 Women's class champion Karen Whittier scored another win at the Black and Blue, but not enough points to beat series champion Sherry Landry.

and under that level water was a few big boulders. Last month's cover shot of Mont Fairfax bouncing up into the air came from that waterhole. Some of the guys managed to get their feet good and wet, while just a couple others took a fair dousing. The most entertaining was Al Zitta, riding in the Super Senior class, who did the stretch-for-life in the middle, as one of his legs got well away from the footpeg, forcing him to either regain balance or teeter into the water. He saved it without dunking, but it sure looked painful.

Later on, everybody tramped through the second spectator spot, in the old gravel pit alongside the highway. Here it was Hines' chance to splatter in front of the crowd, and he did an admirable job of it, laying his CRE down hard right at the bottom of a downhill. Jay Chittenden was excited to be right there and see it, however he was pointed in the wrong direction and didn't get a shot of it. Too bad, we don't often see Hines on his head!

Hines went on to take the win, as we said, but a new face moved into the second place slot. Jason Cayer rode a CRE to second overall and High Point A, barely edging out Mont Fairfax. Both riders scored 20 points, but Cayer saved 23 seconds over Fairfax, and that was all the edge he needed. Third overall was Josh McLevy, who was breaking trail most of the day and came out with 21 points to his credit. Josh was also Honda mounted, and he has a fearless riding style that will make the most hardened spectator gasp in shock occasionally. Josh is going to be the guy to beat in the next couple of years.

B High Point went to Mark Toth, who finished the course with a very creditable 36 points dropped. He was chased home by Gary Szlachetka and Bill Sironen, both of whom dropped 38, with Gary nabbing second in the B standings by only seven seconds. Both the A and B riders covered the entire 90-mile route-sheated course, but the C riders only had to travel 71.5 miles. The High Point C award went to Gerard LaBelle, who once again proved that old age and cunning will win out over youth and exuberance every time. LaBelle rides in the C Senior class, and his total score—the best in the C class—was 40 points.



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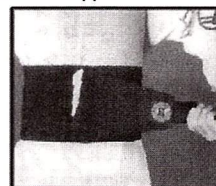
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And so ends another NETRA season. In addition to Hines, Fairfax and Milliken, as we previously mentioned, fourth place for

the year went to Bert Guerrette, who moved to New York in the middle of the season and missed the last five events. He's a dealer

Black and Blue Enduro

Kevin Hines CRE 16

Overall High Point

Jason Cayer Hon 20

High Point A

Mark Toth Suz 36

High Point B

Gerard LaBelle Kaw 40

High Point C

A Bantam

1. Josh McLevy Hon 21

2. Kerry Clark Hon 22

3. Shawn Mason Hon 22

4. Bob White Suz 26

5. Pete Tanner CRE 30

A Light

1. Kevin Hines CRE 16

2. Mont Fairfax Suz 20

3. Paul Milliken KTM 22

4. Kemp Stewart Hus 24

5. Rick Claxton Hon 28

A Heavy

1. Jason Cayer Hon 20

2. James Larabee KTM 24

3. Mike Zahansky KTM 27

4. Pete Tamgini Hon 31

5. Eugene Sweetser KTM 31

A Four Stroke

1. Bill Drummey Hon 22

2. Jim Mitchell Hon 22

3. James Burns Hon 27

4. Rich Seymour Hon 40

5. John Fleck Hon 52

A Veteran

1. Max Parkes KTM 24

2. Ken Robbins Hon 27

3. David Kelley KTM 34

4. Keith Honda Hon 36

5. Mile Kelley KTM 40

A Senior

1. Irv Witkop KTM 27

2. Duncan Broatch KTM 30

3. Fred Burnham KTM 40

4. Dave Ellingwood KTM 42

5. Keith Goodell Hon 44

B Bantam

1. Gary Szlachekta Kaw 38

2. Emory Allaire KTM 52

3. Anthony Mazur Kaw 54

4. Joe McLaughlin Kaw 54

5. Francis Morrison Kaw 57

B Light

1. Mark Toth Suz 36

2. Jim Roberge Kaw 42

3. Jim Cooney KTM 42

4. Kevin Nott Suz 48

5. Chris Tooker Hon 48

B Heavy

1. Bill Sironen KTM 38

2. Tom Hardy Hus 58

3. Mike Stone KTM 63

B Four Stroke

1. Steve Fastert Hus 44

2. Bob Stallard Kaw 53

3. Stan Poplasky Hon 54

4. Kevin Jordan Hon 56

5. John Considine Hus 65

B Veteran

1. Steve Brown KTM 48

2. Bill Riordan KTM 51

3. Larry Smith KTM 52

4. Dennis Gath KTM 56

5. Bob Edwards KTM 58

B Senior

1. Bob Kamay Kaw 48

2. Bob Landry Hon 52

3. Glen Gifford Suz 56

4. Russ McIntyre Kaw 59

5. Bob Young KTM 68

C Bantam

1. Kenneth Besette Kaw 49

2. Steve Mickels Kaw 54

3. Gary Van Voorhis Kaw 56

4. Mark Beauregard KTM

5. Rich Desrosiers KTM

C Light

1. Edward Manley Hon 54

2. David Stoddard Hus 67

3. John Disimone ATK 49

4. Todd Jones Kaw 34

C Heavy

1. Reggie Pichota CRE 58

2. Jeff Scully KTM

C Four Stroke

1. Ken Semerjian Hus 52

2. Doug Morrison Hon 58

3. Thomas Smith Hon 64

4. Paul Gannin Hon

5. Dale Ploski Hon

C Veteran

1. Ken Day Suz 55

2. Jeff Fluckinger KTM 63

3. David Cameron KTM

4. Chris Tsokalas KTM

5. Louis Roth Kaw

C Senior

1. Gerard LaBelle Kaw 40

2. Mark Lucas Hus 45

3. Carl Mickels Kaw 62

4. S. Greg Wibben CRE 64

Super Senior

1. Al Zitta KTM 27

2. Gordon Razez Hon 34

3. Peter Haviland KTM 54

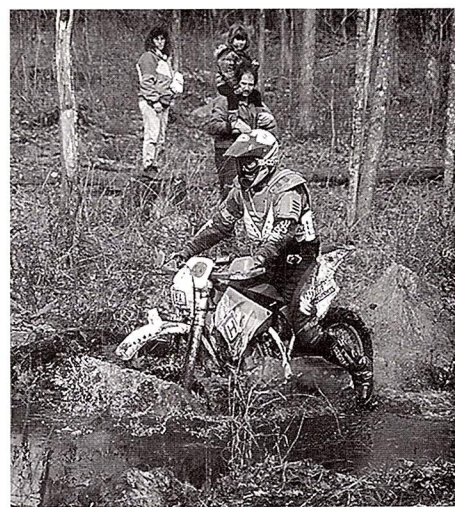
4. Tom Farley CRE 60

5. Alan Ewald Hon

Women

1. Karen Whittier Kaw ck.6

2. Michelle McKinnon Hon ck.5



Kemp Stewart takes the careful line to this stream crossing. Plenty of water kept everyone on their toes, and cool weather made it a little uncomfortable if you were wet. Kemp finished fourth A Light.

rep for Honda now, and he and Laura are expecting their second baby sometime soon. Fifth was Rick Claxton, who spends his days performing magic on suspensions up at Factory Connection. Eric McEachern, who not very long ago was riding in the Junior class, finished sixth for the year, and Dave Gunn finished seventh. Gunn was the NETRA Overall Champion the past two years (as was Milliken before that). James Kelly was eighth for the year, last year's enduro champ Kemp Stewart wound up ninth (after nursing injuries this year), and Darrell

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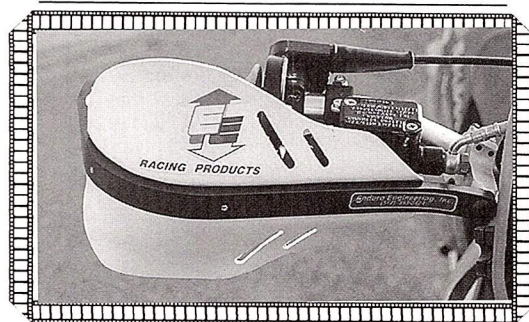
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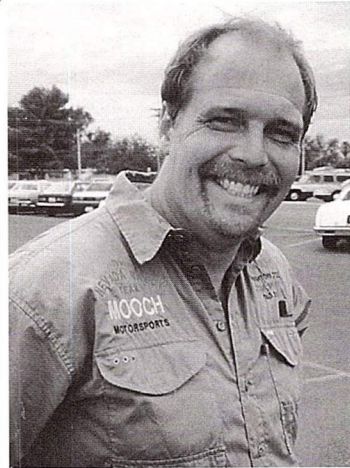
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Tim Ding takes the wet line in the Black and Blue woods. Actually, it was a great day for riding; don't let the water fool you. It could have all been frozen. (Jay Chittenden photo)

Szlachetka finished up the NETRA Top Ten. Do not mourn the end of this season for long, however, because the season opening Snow Run is happening on the 25th of February, and we'll be off and running for a new year. See you out there! □



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by Mark Uth

Warren Grove, NJ 11/12/95

The 1995 ECEA Enduro Season was put to bed this past November with the 25th annual running of the Stumpjumper Enduro. The MCI silver anniversary was marked by below average seasonal weather conditions due to a heavy rain on Saturday that left much standing water on course. The result was plenty of ruts and mud holes throughout the day, coupled with race day conditions that were breezy and cold. Under clear skies, daytime highs in the low 40s were recorded, however it often felt like the 20s due to strong winds all day long. In retrospect, with two feet of snow currently on the ground outside the Trail Rider offices and another blizzard in the process of flexing its muscles, those conditions are looking mighty darn good, and we'd welcome them again anytime soon.

MCI broke in a new trail boss/referee for '95, as gentleman Frank Todash turned the reins over to Rick Ford. This didn't result in much of a change, however, as the '95 course continued to use the classic Stumpjumper formula that included considerable tight single track trail in the AM and much timekeeping in the afternoon. Freshly hewn morning trail was so tight in places that it made any previously run trail, still tight by any measure, seem like highways. There was, however, one twist, as the usual PM points taker was followed by eight-plus miles of new course, rather than the expected two track trail ride back to the fire house. This resulted in an additional late section that took points from nearly all riders. Standing water throughout the day lead to significant rutting and tricky water obstacles.

MCI opted for lengthy course loops, 44.3

ground miles in the morning and 40.5 in the afternoon, that lead to gas planning/fuel range problems for many. This made things tough on riders campaigning converted MXers and some small-tanked enduro bikes, as many riders were seen leaving the

nary a reset for the rest of the day, this confounding any unauthorized pit support to alleviate the gas crunch.

Most of the scoring was done in the AM, as the grueling no reset formula keeps riders wheezing all morning. Once in the woods, actual late sections were often followed by "free time" over open trail/two track road sections that theoretically allowed high speeds to get back on time.

A short points taker soon after entering the woods saw the first points loss of the day. Most fast guys dropped one point at this first check, however, Mike Lafferty and Jeff Kirchner managed to card zeros. Racing to catch up was followed by timekeeping to a check-in at check number five. This was the start of a lengthy morning late section that was to last for more than ten miles. With nothing at stake, Lafferty rolled into the check-in early, for a one minute burn. The ensuing points taker was a major back breaker. Lafferty again posted the day's best score here, dropping nine points at the check-out, followed by the tens carded by Mike McHale, Mark Spence and Craig Cossaboon. From here, riders again raced to get back on time, eventually leading to the midday break back at the fire house. At the halfway point, Lafferty and McHale were tied for the lead, each eleven points down, while Cossaboon, Atkinson, Tomasello, Shenigo, and Crouch trailed a point behind.

The afternoon loop, as expected, started with timekeeping for the first 20 miles or so. Eventually, the course made its way to Coyle Field for the same disappointingly familiar afternoon points taking section, with the same mileage complaints as previous years. Two checks were found



Mike McHale has been waiting patiently for a long time for this, but he finally took the overall at the Stumpjumper enduro by tip-toeing through the burn checks smarter than everyone else.

line with up to a gallon of extra fuel strapped down. Another hardship was the lack of resets, as the only reset all day was the obligatory 0.0+ to 3.1 to keep riders from roaring out from the start. There was



"Honey, don't ride close to the toxic waste dump!" Only in New Jersey could you wonder whether the tape was a joke or not. Regardless, everybody loved the sand pit section, especially the spectators.



Everybody anxiously awaited Joel Dengler's arrival at the sand pit spectator spot, knowing he was going to really launch it...which he did. Even so, he failed to finish the event. But he looked great while it lasted!



Craig Shenigo was the top finisher in the A Medium class; and second A 250 for the year.

within, checks eight and nine, that were burned by none-too-few riders, many again complaining that the club's mileage was off within the section. Mike Lafferty was among those taken, while front runners Spence and Kirchner literally ruined their day, arriving two minutes early at check number eight. Co-leader McHale managed to back off enough to avoid trouble, zeroing the entire lot. While most would have assumed the race was over at this point, there was no



John Roeske was the winner of the A Four Stroke class on his tricked-out KLX250. He also won the ECEA class for the year.

reset after the final check-out, as riders were forced to stay on the gas in a race to the known control, more than 10 miles later.

In a switch from previous years, the course dumped riders into well-worn trail that turned this final segment into a late section. Fast guys zeroed this final push, and, for those who avoided hot points, the entire afternoon loop as well. As a result, it was again the easier afternoon loop that decided the outcome of the race, as hot points played a predominant role. Competition for the overall came down to two morning special tests which culminated with check numbers one and six, and racers who avoided burns in the afternoon.

Mike McHale faired best on this day, with a strong eleven point score in the morning and avoiding the afternoon mine field. McHale's score was good for Overall honors and a one point victory over a trio of twelve

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point finishes. Anthony Tomasello placed first runner-up aboard his YZ250, narrowly edging out Craig Shenigo (third overall) 685 emergency points to 687. Anthony was awarded the High Point A trophy for his trouble. Vet rider Stu Crouch's 12/698 was good for fourth overall seeding, while Mike Lafferty finished fifth overall with a 13. Unfortunately for Lafferty, the day's fastest times in both AM tests did not overcome the afternoon's folly. High Point B went to Vet class rider Lou Green who scored a 20/816, edging out the 20 point scores posted by runner-ups Tom Johnson (20/927) and Dave Maco (20/931). In novice class action, XR mounted T. Britton dropped 28 on the day and earned the High Point C prize, and

another Masters class victory was claimed by Joe Galie Sr.

A fine spread of trophies, plaques and booty was distributed to all top finishers. For those who had less than stellar outings, at least a neat sticker kit was included in the entry package for all. A list of the consider-

able MCI sponsors, who donated prizes and support, is presented in the sidebar. In all it was another admirable effort on the part of MCI club members, friends and family, a fine day in the saddle and worthy finale to the 1995 season. Thanks and accolades are deserved all around. □

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Stumpjumper Enduro

Class Results

Mike McHale KTM 11

Grand Champion

Anthony Tomasello Yam 12

High Point A

L. Green Hon 20

High Point B

T. Britton Hon 28

High Point C

AA

1. Mike Lafferty KTM 13

2. Rich Shirk Jr. Kaw 13

3. Marc Grossman Kaw 14

4. Craig Cossaboon Kaw 14

5. Bill Atkinson Hon 16

A Four Stroke

1. John Roeske Kaw 18

2. Eric Nijkamp Kaw 21

3. Marty Graver Kaw 25

A Heavy

1. Dave Groemm KTM 15

2. Byron Culbertson Hon 17

3. Ken Law Hon 17

4. Tom Folkl Mai 25

5. John Walaczek Kaw 27

A Light

1. Pat Emmons Yam 20

2. Bob Agonis G-G 21

3. M. Dean Spencer Hus 24

4. John Smith Yam 24

5. Vic Chalow Yam 33

A Medium

1. Craig Shenigo Yam 12

2. Frank Vanaman KTM 14

3. Michael Moore Yam 16

4. Mark Spence Kaw 20

5. Larry Poplin Suz 21

A Medium Light

1. John Robbins Kaw 15

2. Steve Reed Kaw 16

3. Rob Morris Kaw 17

4. S. Ober KTM 17

5. Jim McCommon Yam 18

A Senior

1. Dave Barlow Kaw 21

2. Bill Atherholt Suz 25

3. Gary Noble Hon 25

4. Rich Tompkins Hon 29

5. Jay Gillfillan KTM 29

A Super Senior

1. Jack Lafferty KTM 22

2. Roy Fliegau KTM 23

3. Rich Trader KTM 28

4. George Clickner Hus 31

5. Pete Parlett KTM 32

A Veteran

1. Stewart Crouch Hon 12

2. John Neifert Suz 19

3. Dave Bostrom Kaw 27

4. R. Piascinski Hon 45

B Four Stroke

1. Rob Kirkpatrick Hon 22

2. Frank Lillo Hbg 27

3. Blair Kolbeck Hon 27

4. W. Mason Hon 31

5. Ron Murhon Jr. Hon 31

B Heavy

1. T. Johnson KTM 20

2. Jack Lewis KTM 24

3. James Reber Kaw 26

4. Rick Kiveb KTM 29

5. George Potts III KTM 30

B Light

1. C Copeland Hon 23

2. Steve Fox KTM 26

3. S. Brown Suz 41

4. N. Sotiropoulos Hus 49

J. Shanline Kaw 50

B Medium Light

1. Ron Lucas Kaw 26

2. D. Moorehouse Kaw 28

3. M. Bilunas Kaw 37

4. J. Castaldi Kaw 38

5. Eric Corbin Kaw 41

B Medium

1. Peter Wright Suz 22

2. M. Bradway Suz 23

3. M. Sigety Suz 24

4. Bob Solomon KTM 25

5. Chris Eyrick KTM 26

B Senior

1. Kim Leary Hon 26

2. J. Diobilda Kaw 33

3. Steve Hyde Yam 36

4. Mark Trowbridge KTM 38

5. Ronald Eder Kaw 46

B Super Senior

1. J. Schwarz Kaw 27

2. William Parrish KTM 35

3. B. Schmidt Hus 50

4. R. Darpino Kaw 56

5. K. Buchel Kaw 62

B Veteran

1. D. Maco Suz 20

2. Rob Kozacheson KTM 25

3. Dave Uth Yam 27

4. Scott Dagrosa Yam 27

5. Joe Kelly Suz 28

C Four Stroke

1. R. Trout Hon 77

C Heavy

1. W. Lihey Kaw 60

2. T. Blassoyk KTM 107

C Light

1. R. Nicholson Kaw 34

2. T. Quinn Kaw 35

3. D. Frazier Kaw 38

4. T. Gallagher Kaw 46

5. D. Little G-G 52

C Medium

1. M. King KTM 50

2. R. Heisler Yam 56

3. B. Torsiello KTM 58

4. Chris Netekios KTM 63

5. Jim McCullough Kaw 69

C Veteran

1. R. Comber Kaw 35

2. D. Hurley Kaw 46

3. D. Chambliss KTM 55

4. Keith Krause Suz 56

5. Mike Barr Kaw 58

Women

1. N. Davidson Kaw ck.3

2. H. Glynn Kaw ck.3

Masters

1. Joe Galie Yam 45

2. Dan Van Driel Hus 69

3. Ed Baker Hon 129

4. Rob Hoover Hon 140

Teams

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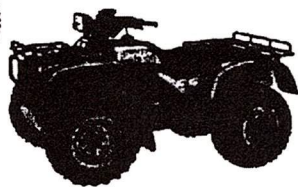
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KTM 125E/XC

If you like them light and easy to ride, here's a pocketful of fun

Should you be riding a 125 enduro bike? For a lot of people, that's a tough question to answer. Sure, if you're just starting out or weigh-in at 125 pounds, it's a natural—a 125 will work great, and won't be so powerful to intimidate you. But what about the rest of us, all the 200-pounders who should know better than get on a 125? Plenty of big people and Senior class riders are looking at 125s these days, for many valid reasons.

1. They're cheap. Even though it costs the same to make a 125 or a 620 (at the factory level), manufacturers are forced by the market to price a 125 lower than a big bike. This KTM 125 we're testing this month carries a suggested retail price of \$4548, which is almost \$1000 less than a 250. For that \$1000, all you're getting less of is cubic displacement; every other feature on the bike is the same as the big bikes.

2. They're light. Compared to a 250 or Open bike, a 125 always weighs less. In the

case of the KTM, the 125 E/XC weighs exactly 20 pounds less than the 250/300/360. Twenty pounds may not seem like much, but it's a big deal when you're tired or need every last bit of advantage to get through that tight spot or drag your bike out of a swamp. Weight is all important; if you don't believe us we'll make you race a 300-pound bike for a while.

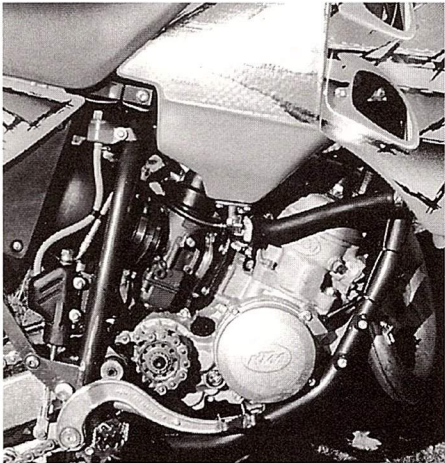
3. They're not gutless anymore. Modern 125cc enduro bikes are an amazing combination of wide powerband and strong midrange power, while still delivering that characteristic 125cc shriek on the top end. If you think 125 machines are still horribly peaky and unridable, you need to sling a leg over a 125E/XC or a Husky 125WXE one of these days soon, and see what you think. We're almost positive you'll be shocked at the available power.

Armed with these revelations, we hopped on a 125E/XC for a day this past fall, and were suitably impressed. First of all, the 125E/XC is easy to start—actually that should be a main feature on the above list. You push the kickstarter down with a half-hearted jab and the bike lights up like it was running all along, just not making any noise. The only complaint we could possibly have is the small size of the kickstart lever. It appears to share the same part number as the shift lever (joking—don't take that as truth), and if you have size 11 feet, sometimes you're not sure you've got enough of your big toe wrapped around the kicker to push it down without it slipping off. If you have reasonable size feet this isn't that much of a problem.

Jetting is everything on a 125, and this bike is no exception. Like a lot of KTMs, the 125 seems to come through jetted a little strange; we found it to be a little fat on the bottom and lean on top. Our jetting was dialed in by H&H KTM in Douglasville, Georgia, and for cool fall weather they had the bike jetted



Way too much fun. See how much the back wheel is spinning? That's what it takes to ride a 125 fast, and this KTM is definitely ready to spin.



KTM's "secret" 125 enduro powerplant. The secret is, nobody knows how good it runs in the woods.



Details appear largely the same on the 125, with the exception of the butterscotch color. You either love it or hate it. We were hoping for purple.

with a 215 main, a 40 idle jet, a K55 needle in the second position, and a DP262 needle jet. For the day we rode the bike, this was perfect jetting. The starting jet stayed stock at a #60 jet, and like we said the starting was flawless.

Once the engine is warm, the first thing you notice is the abundant low-end power. Yes, it's no 250, but you can ease the clutch out and move right along with this bike. No frantic revving and clutch-slipping required. The good bottom end eases right into a quick midrange hit that tugs the bike up to speed. When you hit mid revs you have a decision to make. You can shift up and gently ease up in speed, or you can pin it and start riding like a 125 knucklehead.

Which way you go mostly depends on terrain. If you're riding on level ground, you can do whatever you want. You can sit down and short shift all day long, and just have a nice mellow ride on a light bike. However, if the terrain points up, or if you're racing somebody for keeps, you'd be better off hanging on and letting the KTM buzz, which is can do quite well. Pin the throttle and the 125 gathers it's strength and then leaps forward, and by the time you get to about eight grand it's begging for another gear. Keep it pinned and smoothly jam it into the next gear and the bike leaps forward like a beestung jackass, and if you're not paying attention the back end is going to hop out



SETRA hare scrambles star Alan Gravitt churns up the Georgia woods on our 125 test machine. You give up some horsepower with a 125, but small bikes are so much fun.

shift down two gears coming into a corner or hitting the base of a hill, rather than just taking one, as you'd do on a 250. When all is said and done, riding a 125 aggressively can be a lot of hard work for your left foot, but the light weight and low inertia of the engine makes up for it.

This year's KTM is coming with the new Magnum 45 forks, which so far have proven to be a major improvement over the '95 forks, which we had mixed feelings about. The new Marzocchi Magnums seem to be valved a little more accurately for the spring weights Marzocchi likes to use. We never got a chance to tear them all apart and see what they're doing, but we did notice that the forks work better, right out of the box, than they did last year. Roots and hard-edged ruts are swallowed up with very little stress, and choppy, rough terrain doesn't know the forks out of your intended line. We like them, much better than last year, although we would recommend that everybody open them up when new, change the oil to a good cartridge oil and make sure the oil level is set accurately. We'd recommend this procedure on any bike's forks, not just KTM. You'd be surprised at how far off most fork oil levels are, even straight from the factory.

of line when the rear wheel starts spinning. That's okay though, because on a 200 pound bike all it takes is a twitch from your back end, and it snaps back into line. Pin it, rev, repeat, until you run out of gears!

In the really tight, twisty stuff the low-end power definitely helps, but in hills you have to ride aggressively because yes, there's low end, but it's still a 125. If you're not used to it the first couple of ride might be frustrating, but seasoned 125 riders soon learn to

The rear suspension is provided via an Ohlins Type 3A shock, and it is confidence-inspiring to know that this is arguably the best shock absorber made in the world. The KTM engineers have done more testing



We were much happier with this year's forks, compared to last year. KTM and Marzocchi have massaged the forks to work even better off road.

since '95, and as a result the rear shocks now come with even better damper valving, which is not to say that last year's was bad. This '96 just seemed to be a little more supple in the initial part of travel, and felt cushier without being "soft." Nice rear suspen-

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sion; no hopping, no kicking up, just good control of the back wheel. However, like the forks, if you're really serious you'll let a competent Ohlins service person change the oil on this shock after break-in, and make sure that it's filled and bled with the proper oil. Nine times out of ten bad rear shock action can be traced to air in the shock or other assembly errors, and if you want to treat yourself to the best you won't take any chances.

If the 125 feels smaller than the 250, it's no illusion. The KTM 125 frames are slightly smaller; maybe an inch and a half in wheelbase. Don't expect a low seat height, however, because the 125 boasts of a seat height of 37.4 inches—two tenths of an inch taller than the 250. The seating position is typical KTM—you feel like you're sitting on top of the bike, with plenty of room between you and the handlebars, and a good relationship between the seat and footpegs. The frame is chromoly steel, while the rear subframe is aluminum.

Putting it all together and threading this bike through the trees is a blast. It feels like you're riding a mini bike, and when you find the sweet spot in the powerband you can blast all day, hopping between turns and just plain having fun. Yes, you do have to shift more, but that's part of the trade-off for all this light weight and flickability.

Incidentally, if you think you want to the 125SX instead of the E/XC, this is what

SPECIFICATIONS KTM 125 E/XC

Engine Type:	Liquid-cooled 2 stroke
Displacement:	124.8cc
Bore/Stroke:	54.25 X 54mm
Transmission:	Six-speed WR
Gearing:	13/48
Chain:	Regina o-ring
Tank Capacity:	9 liters (2.4 gal.)
Carburetor:	Dell'Orto 37mm flat-slide
Ignition:	Digital CDI 130w lighting
Forks:	Marzocchi Magnum 45
Suspension Travel:	300mm
Front Brake:	Hydraulic disc
Front Tire:	Michelin MP11 90/90X21
Rear Suspension:	Ohlins Type 3A
Suspension Travel:	345mm
Rear Brake:	Hydraulic disc
Rear Tire:	Michelin MP11 120/90X18
Seat Height:	949mm
Wheelbase:	1430mm
Ground Clearance:	381mm
Claimed Dry Weight:	208.3 lbs.
Suggested Retail Price:	\$4548

you'll find: The SX weighs 10 pounds less (198.4 pounds) because of a different rear subframe and no lights. It has a PVL internal flywheel ignition with no lighting coils, and a close ratio transmission that has a taller first gear and lower sixth gear than the E/XC. It also costs about \$150 less than the E/XC. Everything else is essentially the same.

Well, we only had a day on this little popper, but we wish it were longer. As a matter of fact, we wish we had one of these things in our garage. The fact that this bike is as much fun to ride as it is stands as the answer to the "Do I want a 125?" question. If you like light weight and maximum maneuverability, well yes, you probably do. This KTM 125E/XC has an excellent suspension, KTM's legendary handling, and an amazingly versatile engine. You might have to ride it a little bit harder than a 250, yeah, but along the way you're going to have a ton of fun. □



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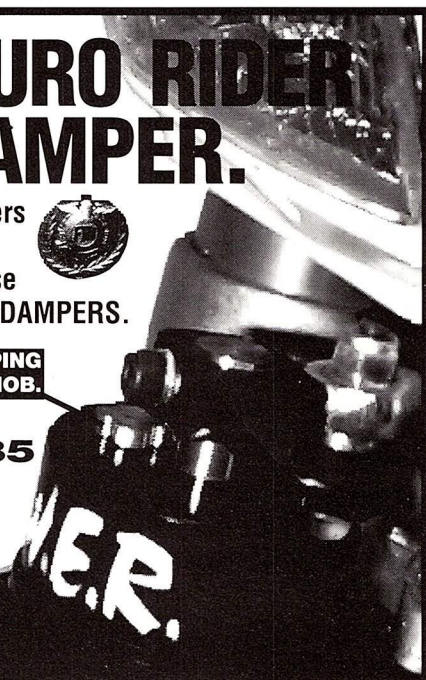
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Editor Tom Webb's list of racing credits includes AA National enduro rankings as well as International Six Days Enduro experience. You know those riding fantasies that most of us have? Tom's fulfilled them. And he backs it up with equally impressive journalistic style.

Editorial Director Charlie Morey has been riding since 1965. His knowledge and perspective on today's political and land-use

issues are equally formidable.

Test Editor Karel Kramer hopped on his first bike in 1965, and he's become our walking encyclopedia of technical knowledge about every machine produced since.

Mark Kariya began riding in his dad's orange groves back in 1965. Since then, "Kato" has ridden and raced literally every type and size of motorcycle known to man.

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LEMBO LAKE

Norton clinches it in the last tough mudder of the year

By Cheri Alix, photos by Clipper

Modena, NY 11/19/95

After two incredibly muddy races, everyone was hoping for the final race of the year to be nice and dry. After 28 inches of rain in thirty days, three inches of which fell the night before, a dry track was not to be. As we pulled into the parking lot, traces of snow could be seen in the woods and the supercross track was just plain mud. It looked like it would be another disaster, but



Scott Phelps had the worst day. He slid into a tree first thing, and tore off his rear brake, ending his chances for a repeat of the number one plate.



Todd Levesque with the classic nose-down supercross jump, skimming through the motocross track. This short section was the most fun for all the riders.

it turned out quite to the contrary.

Lembo Lake has become known as one of the funnest tracks in the series, and this weekend would be no exception. The club laid out a 4.5 mile track using a variety of terrain. The start was in a large field at the back of the apple orchard. The riders came off the start and weaved through some of the apple trees and out onto the supercross track, which is about the size of an arenacross track, then into the woods for about one and a half miles before coming back into the orchard. Even with all the mud, the rocky ground remained solid, making the entire track gnarly but passable.

The Junior event started off the day at 9:30 with a three lap race. Immediately taking over the lead was Yamaha rider Doug Stroh, Jr. His lead would be short-lived, when on the second lap he made contact with a non-forgiving apple tree. The lead was then passed onto Brian Lawson who held on till the end, taking first after a long battle with second place finisher Matt Jalbert. In the Mini class, it was Mike Peristere edging out Drew Carpenter for the win. Honda rider Michelle McKinnon took first place in the Women's class with Pamela McCann in second.

The Novice race had one of the smallest turnouts this year with only 88 riders, many of whom were local riders. After six laps of racing it was a local rider taking the overall. Suzuki 125 rider Gene Scott took the win with only twenty seconds over second place finisher Matt Bruce, also on a 125. Novice 250 rider Joe Senecal, took the 250 class win and finished third overall for the day.

With many class titles and the NETRA Championship still on the line, just about everyone showed up for the final event, almost 140 riders in the Amateur/Expert ranks. The main battle of the day would be between Dirt Works rider Scott Phelps and SCR/Yamaha/Team Mirage rider Tom Norton. There were twelve riders lined up on the front row. The flag went up right on time. All that was seen from the remaining rows of riders was flying mud, but around the corner the spectators could see that it was Phelps out in front with the holeshot. Following Phelps onto the supercross track was PJ Peculis, Jack-Bob Speroni and Norton.

Unfortunately, Phelps wasn't a quarter-mile into the race when he sideswiped a tree, ripping his rear brake lever clean off and ending his day. His only chance to take the title was if Norton also DNF'd, but that was not going to happen. The Scott/Bel-Ray/Valley Motorsports-sponsored Norton was on a mission, and nothing was going to stop him. Within the next half-mile Norton moved passed Speroni and Peculis to take over the lead. By the end of the lap,

he had a 17-second lead over Yamaha rider Peculis. Speroni was still holding onto third followed by Dave Gunn and Josh McLevy.

The short course was really taking a beating. The woods were now rocky, rooted, muddy with water-filled ruts, totally gnarly.



A Little deep, you say? This is the starting line, and Norton almost didn't need the foot-stand to start.

But no one was complaining; they were having a ball! Especially in the field sections which were completely worn in. You could just rail around the berms. The lap times, which were around 21 minutes for the Novices, were now down to 17 minutes for the leaders, with the second lap being the fastest of the day at 16 and a half minutes.



Jack-Bob Speroni does the slithery-slide into a corner in the apple orchard. It was muddy there, but it was worse in the woods.



Did anybody fall, you ask? Oh yeah, every now and then. Most DNFs were from riders who just got tired of trying to stay upright.


It was still Norton in the lead at the end of the lap, and Peculis not 25 seconds behind in second. Moving up on the two riders was Honda 125 rider McLevy, passing Speroni to take over third place.

By the end of the third lap, Peculis had closed the gap on Norton, allowing him to take over the lead when Norton stopped for gas and goggles. Norton soon returned the favor not a half-mile later, taking back the lead and putting 50 seconds on Peculis by the end of the fourth lap. Peculis stopped for gas at the end of the lap, allowing a hard pushing McLevy to take over the second place position.



Another flying shot of "fearless" Josh McLevy, who finished second overall and very close to a real battle with Norton.

Norton kept the hammer down coming in at the end of the fifth lap with over a minute and a half lead on second place McLevy, who had another minute on Peculis. Norton continued to pull away, putting another three minutes on the pack by the end of the seven-lap race, finishing just under five minutes ahead of McLevy. It looked like Peculis would hold onto third place till the end, when a hard-charging Todd Levesque slid by, taking third by only a second ahead of Peculis. Rounding out the top five was the Hoot Owl club leader, Ken Law on his Yamaha 250. Chris Crispin physically crossed the finish line in fifth place, but Law beat him in adjusted time. Law also took the Expert Overall, finishing almost four minutes ahead of the next Expert rider, Rory



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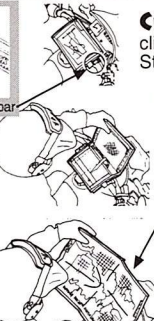
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
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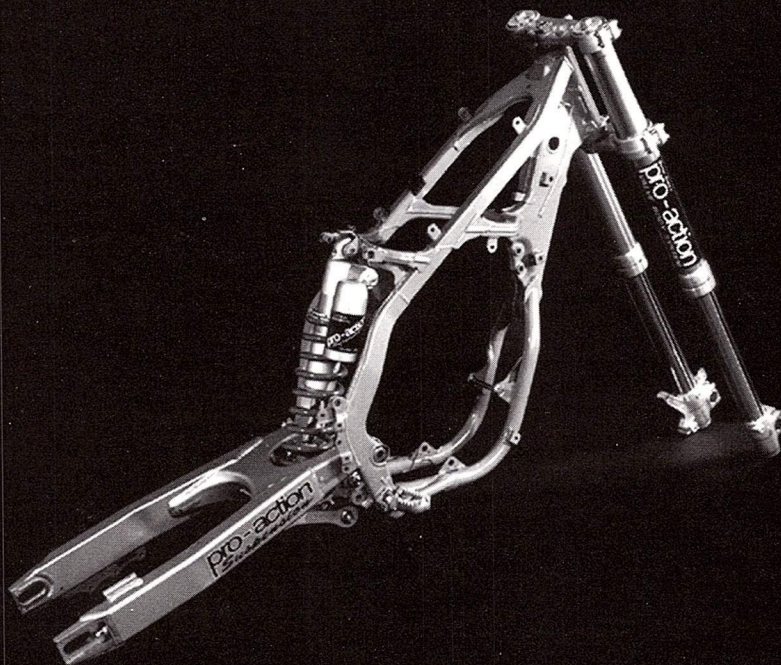
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Eastman. Eastman, who already had the Vet class title wrapped up, decided to compete in the 250 class and took first place, with over a minute over second place Arthur Menzel.

Taking the class win in the Vet class was new inductee, Russell "I'll be gentle" Bain, followed by VP Racing's Norm Turnberg. Waging a heated battle for second place in the year-end standings were Expert 125 riders Luke McNeil and Craig Vollkommer. With Brian Tucker wrapping up the class win at the last race, it was a battle for second between the two riders. McNeil was down four points going into the race and needed to finish in first or two places ahead of Vollkommer to take second for the year. After a hard fought battle with both riders changing positions continuously throughout the day, McNeil came in second for the day with over 30 seconds on Vollkommer, but finished third for the year down one point to

Start of the AA class shows McLevy getting wild on the outside, and Phelps in perfect holeshot position, with Randy McCann trying to find a hole on the inside. Norton is almost in last place here.



Lembo Lake Hare	
Scrambles	
Class Results	
Tom Norton	Yam
Overall Champion	
Ken Law	Yam
A Overall	
Ernie Leclero	Suz
B Overall	
Gene Scott	Suz
C Overall	
AA	
1. Tom Norton	Yam
2. Josh McLevy	Hon
3. Todd Levesque	Yam
4. PJ Peculis	Yam
5. Chris Crispin	Hon
Junior	
1. Brian Lawson	Yam
2. Matt Jalbert	Yam
3. Scott Robert	Hon
4. Derek Carpenter	Hon
Mini	
1. Mike Peristere	Yam
2. Drew Carpenter	Suz
3. Dan Young	Yam
4. Nate Kanney	Yam
5. Joe Mullaney	Hon
Women	
1. Michell McKinnon	Hon
2. Pamela McCann	Kaw

3. Amanda Young	Kaw
4. Kim Merchant	Hon
Novice 250	
1. Joe Senecal	Yam
2. Clyde Wensin, III	Yam
3. Mike Corri	Hon
4. Kyle Vieira	Hon
Novice Senior	
1. Larry Piers	Suz
2. George Olsen	Hus
3. Kerry Koeller	TM
Novice Open	
1. Mike Calderon	Kaw
2. Kevin Leeds	Hon
Novice 200	
1. Todd Quinn	Kaw
2. John Lawrence	Yam
3. Tom Vallee	Kaw
4. John Matte	Kaw
Novice Four Stroke	
1. Chris Cramer	Hon
2. Pasquale Gigliotti	Hon
3. Robert Weiss	Hon
4. Pat Fogarty	Hon
Novice Veteran	
1. Bill Wilson	Hon
2. Eric Reinhard	KTM
3. Jeff Heino	Suz
4. Dave Drenutis	Hon
Novice 125	
1. Gene Scott	Suz

2. Matt Bruce	Hon
3. James Menard	Yam
4. Dave Kanney	KTM
5. Dennis Phelan	Kaw
Amateur Veteran	
1. Ray Van Guilder	Kaw
2. Ken Held	Kaw
3. Gordy Waters	Hon
4. Paul Campbell	Hon
Amateur Open	
1. Glen Dougherty	KTM
2. Steve Anderson	KTM
3. Ed Sekelsky	Suz
4. Tom Quaka	Kaw
5. Chris Wyckoff	KTM
Amateur 250	
1. Dave Vallee	KTM
2. Dave Devine	KTM
3. Thomas Roe	Kaw
4. Pete Byrne	Yam
5. Robert Carlson	Suz
Amateur Super Senior	
1. Thomas Marsh	Suz
Amateur Senior	
1. Bob Young	KTM
Amateur 200	
1. Chris Simpson	Kaw
2. Fred Alden	Suz
3. Garson Smith	Kaw
4. Mark Hamilton	Hon
Amateur Four Stroke	

1. Bret Thompson	Hon
2. Gus Bender	Hon
3. Bob White	Kaw
Expert Four Stroke	
1. Dave Simcock	Hon
2. Kevin Paine	Hon
Expert Open	
1. Doug McKinnon	KTM
2. Jim Simcock	ATK
3. Michael Rae	KTM
Expert 250	
1. Ken Law	Yam
2. Rory Eastman	Hon
3. Arthur Menzel	Kaw
4. Mike Lewis	KTM
5. Guy Robery	Kaw
Expert 200	
1. Brian Tucker	KTM
2. Luke McNeil	Kaw
3. Craig Volkomer	Suz
4. Brett Costello	Yam
5. Bill Pybas	Hon
Expert Veteran	
1. Russell Bain	Hon
2. Norm Turnberg	Yam
Expert Senior	
1. Wayne Fontanazza	KTM
2. Jerry Lynn	Yam
3. Jerry Harris	Yam

Vollkommer. Tucker took the win for the day coming in another 30 seconds ahead of McNeil.

Taking the Amateur high point was Open class rider, Ernie Leclero on a Suzuki. The only heated battle of the day in the Amateur division was in the Vet class, with Ray Van Guilder taking the class win

with only 20 seconds on second place Ken Held.

The Tri-State Motorcycle Club put on a great event. From the persistent efforts of Roger Billharz, the club received contingency certificates from such manufacturers as Spectro Oil, Factory Connection, White Bros. and Maier plastic, which were given out to class winners. We all hope to see Lembo Lake on the schedule next year, but maybe during a warmer weather! □



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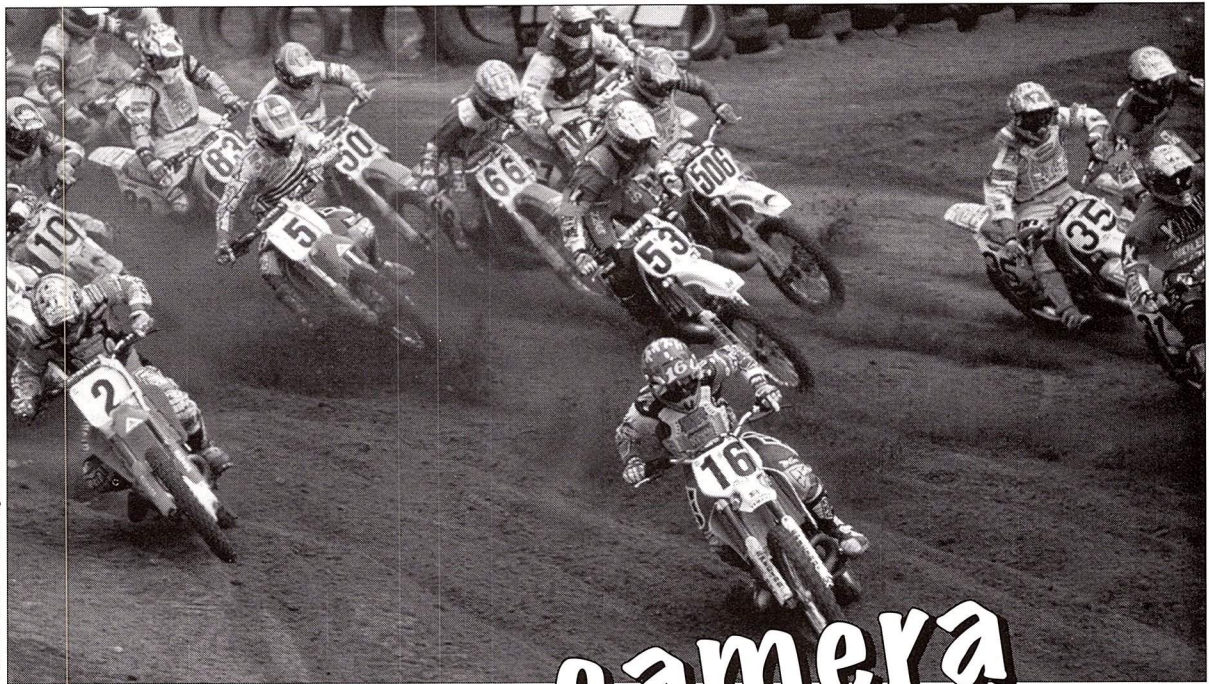
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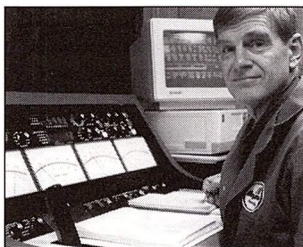


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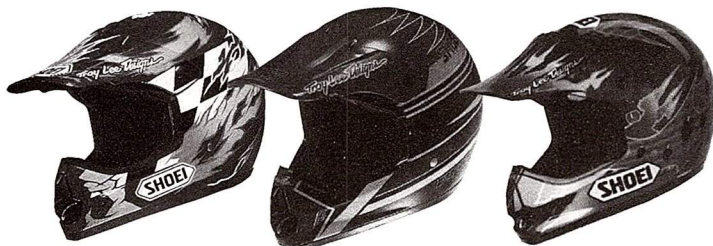


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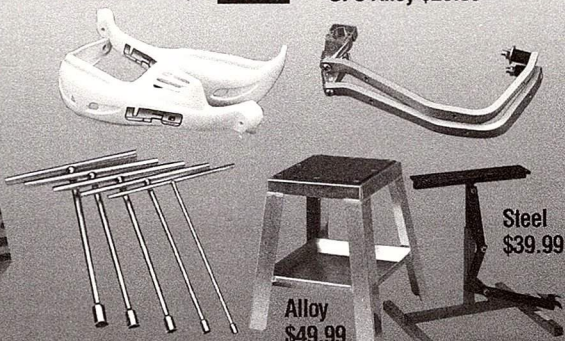
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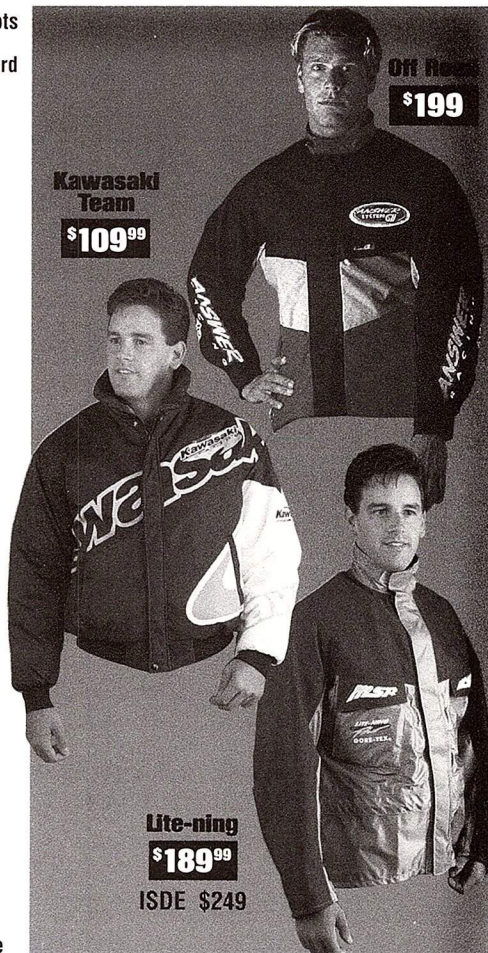
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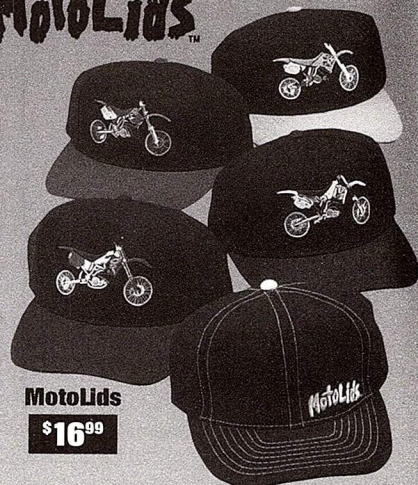
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FEAST AND FAMINE

The GNCC Finals: The last race in Crawfordsville, Indiana, and the awards banquet for '95

by Charlie Williams
banquet photos by Davey Coombs

Indiana is a new venue for the GNCC, maybe the farthest west this growing series has been. You would have thought the race track had been there for years, the way Racer Productions dresses up a bean field. Flags, banners, ribbons, P-A system, a carnival-esque atmosphere. Today's the day it will all be decided, Scott or Scott. My class had already been decided, so it was just a matter of showing up and riding, no racing for me, hadn't been much all year. Richard Bryan would beat me by 20 minutes for first place, I never figured out which guy was second place, I got third and six day medalist Greg Holden finished fourth, usually a few minutes behind me. This last day in Crawfordsville, Holden was still nursing a broken arm sustained during the prestigious Nevada Rally. So the challenges of racing for a final position was nonexistent, a ride with no pressure: fine with me. I was wrong again. Crawfordsville is only 40 miles from my residence, the Canteen Sanitarium, and every last person I know showed up to watch me beat up on Scott Summers.

"We're gonna bring the vidiot camera and git movies of you racin'! Hey, sorry about the broken glass and the blood, TJ's passed out now so he ain't gonna be no more trouble. Hell, we came to watch racin', not fight!"

"Thanks Dad, I'm sorry I don't have any more Mooch shirts for you and the guys."

The announcer interrupts us with the offer of a free manicure or free gift from the Mooch pits. Mooch pioneer Jonas Vance had rallied up the entire business community of his home town, Greencastle, Indiana. He had mooched over 1400 gift items, from standard ball caps and can coolies to more obscure prizes like complete ladies' outfits, massages, haircuts and on-the-spot manicures. My toenails look terrific! Mooch sponsors, Godby Brothers and Dixie Chopper, should feel proud and well-represented with the efforts put out by Jonas, and the support from his town. Who knows, he may be the first rider to get sponsored by his home town chamber of commerce! Tommy Norton got a spastic cartoon turtle to sponsor him, and I'm still trying to crack that Tucks factory ride.

Anyhow most eyes were on the front row. Scott Summers versus Scott Plessinger, winner takes all. Third year in a row the series was decided at the last race, the crowd was bubbling with barley-fueled excitement. More announcer babble, more family and friends show up, more cuticles get pushed.

Old rivals show up and sneer intimidating toothless sneers, trying to psych me out. Today's race won't be for class position but a more prestigious prize: pecking order of peers. The classes were peppered with Hoosier cherry pickers. There were Six Day multi-gold medalists riding in the "C" class and other Six

Day finishers in the "B" class. Hang your 8th place B class trophy next to your Six Day medal? Hello? I'm sure this is going on in other states, but come on! Some C class rider got beat out of a sixth place trophy by an ex-Trophy Team rider. How do you feel about yourself now? What if I had used your real name? (Please note author's self control, he could have totally and righteously ripped on all parties involved in such blatant trophy grabbing.)

Plessinger wound up beating both Summers and myself for the championship. I had a pretty good race, the course was easier than some of the West Virginia courses, but ridden hard enough and fast enough, it was plenty brutal. I got to race with Illinois ace Gordon Bradfield. Gordo is the leading force in the Illinois chapter of Moochism. Dirt Rider's Tom Webb, series Senior champion, caught me, politely passed, and then just rode away from me. I hate that, when that happens. I caught my friend Bryan Stoughton, he had fallen and thought he had rebroken his glass collar bone. "Heh heh,

ator geyser pit thing. Eventually I was back in the race trying as hard as I could go to catch my buddy, ex-national open A hare scrambles champion Bill Cavanaugh. He was the next rider ahead of me in my class, so it was a legitimate race and not just a battle between friends. I also was trying extra hard so I would not get lapped and miss the final lap, so in a way I was racing Plessinger and Summers (You're stretching it here Charlie...).

So late in the last lap I was just flying along. I just knew Bill was just ahead, I was really pushing it. I don't know why it is so much more fun to race someone you know, maybe it is because you can share it again later. But I could see his dust and smell his exhaust, I knew I was catching him and if I could surprise him...if, if, if.

I didn't get the chance. Miscalculation, brain fade, stupid mistake? I hit a tree, hard, first with the right handlebar way in where the bark buster mounts to the bar. The force broke bones in my right hand and shot me across trail into another tree. The bars were ripped



Snapshots from the banquet, from upper left: Tommy and Cheri looking bored without racing to do, Fred 'n Amy Andrews, Brian Keegan and Jim Kelly's significant other, Tim Shephard, all the boys lined up for their prizes, Scott Summers and Fred Bramblett, Scott and Angie Plessinger and Danielle, who's waiting for her new brother.

passed him like he was sitting still."

I had been having problems with my bike overheating and could not pinpoint the problem, so at my gas stop, Dad was pouring gasoline in my lap and I reached down to the radiator cap and gave it a little nudge to see if it would, you know, come off. Well yes, it came off and it spewed boiling antifreeze 20 feet in the air, burnt my hand, again. Mom stood back and watched, nobody else was doing the radi-

back the other direction and threw me up and over, head first onto the ground, the bike cart-wheeled over me and stopped. I laid there for a long time on my stomach.

I could tell I was alive. I could wiggle my toes. I was okay except I could not see. I had been knocked blind...well not totally. I could see some sort of terry cloth background with music notes dancing, with bowling pins rolling over and over. With great effort I rolled on to my

side, still blind, but I could smell the sweetness of the dirt impacted in my face guard. I could hear too. I heard a four-stroke approaching slowly, then stopping, shutting off with the noise of the kick stand squeaking down. "You all right?"

"Yea, I'm okay. I've just knocked myself blind. I'll be okay, go ahead, I'll catch up."

I could feel him rolling me over with his foot, on my back now. "You okay?"

"Oh yeah, I'm fine, I've just temporarily knocked myself blind, you go ahead, I can handle this, its just a little blindness."

The stranger roughly grabs the face guard on my helmet and pulls it up. My vision is restored! Sure, the pop corn boxes are still doing the doe se doe with Linda Vaughn, but I could distinguish light from dark. What I had been seeing was the liner of my helmet when it was rotated forward on my head. Later my shoulders would bruise where the helmet dug in and my throat hurt from the extra stress on the strap.

My vision was clearing. I made it to all fours, still dizzy and seeing starfish chase snowflakes across my field of newly restored vision. I got up and walked

over to my twisted bike, the bars were bent, the bark busters hung straight down, the sub frame was twisted around and the rear fender was broken. I feel confident my mind had been

jostled. I limped back to the pits, my fore head would later bruise and swell from the crash.

The next afternoon I stumbled around lost in the Miami airport, seriously questioning the sanity of going to a third world country with all this forehead swelling, nausea and grogginess. The flight promised lunch so I took that as an omen and wheeled the stolen wheel chair aboard.

The next time I let the thought of motorcycles enter my traumatized noggin, Clipper was on the phone.

"I'll go if you go." He was talking about the GNCC awards banquet.

"Okay I guess. Hell, spawning season is over, might as well drive two states away and hang out with a bunch of people I hardly know." So it was set; the 1995 Trail Rider summit meeting, a think tank...more of a dip tank, really. Paul brought his friend Lindsay, or rather Lindsay brought Paul—see the mooch? Lindsay

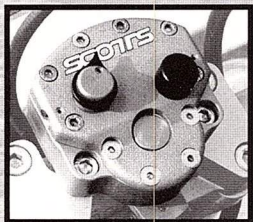


More pictures of people and food: top row, C.J. and Jeff Russell, Rita, Dave and Davey Coombs, cool motorcycle shirt, Holiday Inn girls, dueling bartenders, Kim works for Davey, Charlie hard at work on a new story, buckets o' grub.

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Scott Plessinger
Grand National Cross Country Champion

Background Photo is of Larry Roeseler. Scotts Damper is also the official Steering Stabilizer of Team Green.



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works in the construction of racing motorcycle fuel tanks. He makes them with the dry break fill spout. Paul and I wanted him to design a dry break that installed in the back of your neck for quick refills of Yoo-Hoo. Then our mouths would be free to talk about our favorite subjects: us.

The banquet was held at the Holidome. This was neat because everyone stayed overnight, and got drunker than billy goats. I got to strut around the pool in my Speedo showing off my new tattoo. It is an alligator eating a golfer using my navel as the mouth. I went to the workout room, sat on the weight bench and ate chips and watched exercise on TV. Now I could get used to GNCC events like this. Hell, leave the bikes at home, let's all meet at the Holidome and get fist-fighting drunk!

No not really. It was really a very nice crowd with many pretty girls. Dinner was an elaborate mixture of unidentifiable meats and starches. Paul introduced me to Bruno. Paul informed me that he and Bruno had spent a lot of time together on the continent. I turned to Bruno, not realizing how little English he spoke, and said, "So Paul tells me you're incontinent." Bruno just smiled and nodded his head politely.

Paul and I were making fun of how small some of the guys were. I said both Norton and Plessinger could fit in my pants. That's just what I need, another Ninja Turtle in my pants!

Okay, enough beating around the bush, let's get right down to the venomous acceptance speeches by the top riders. Andrews brought up the fact that if they counted all the races he would have won on consistency. You are not going to get any praise out of Andrews. He is a tough character, with a gunfighter's attitude. Summers dressed as a mortician, admitted he tried as hard as he could all year and was disappointed knowing he would have to train

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even harder next year. Gad, any training at all upsets my stomach. Plessinger is the king of cool; he flat tells it like it is. Scott said, "You gotta do it twice to make a man out of you," obviously referring to Andrews single championship. Summers stood at attention nodding in agreement and laughing at the humor, the room roared with laughter, and the very best part was Andrews was laughing too. This scene alone was worth the trip.

Racer Productions really gives out the prizes.

Stacks and stacks of stuff, the most impressive may have been the 40 sets of tires Dunlop gave away. I got a plaque with my own name on it, a pair of AXO gloves, a Link Drink bottle, a gear bag from CTI the knee brace people. I received discount coupons on Dyno-Port pipes and Wiseco pistons, and a free massage somewhere in South Carolina. I got a cool shirt from KTM that says "Achievement Award," and a KTM hat. A pair of hand grips and a genuine pair of Fredette Hand Savers. Very nice, thank you Jeff. I got a pair of tubular compression bandages. Beats me, they are way too big to fit on the piece of anatomy I was thinking of. I got a new set of tie downs and a free copy of Racer X. This is a must-read paper light years ahead of its time, compared to the few other motorcycle publications available. Except of course Trail Rider, especially since Vogue voted Trail Rider one of the best underground maga-

zines available. You always dreamed of being in on the ground floor of a cultural catalyst, here is your chance, right in your hands, kick back and smile, you now journey with the all-knowing.

The next morning they had a riders meeting. Representatives from the various manufacturers lobbied for rule changes that would help their brand of machine. All were present but Plessinger, he arrived late in his wife's clothes; he may have started styling his hair like I do, too. Anyhow, he kicked back and agreed with everything mentioned. "Yeah, sure, that's okay, I don't care, any way you want to do it is fine by me. You want me to ride a moped? Okay, whatever you say."

It sounds as if there is going to be even more competition next year. Rodney Smith, Ty Davis, and Keith Johnson will be racing the full series. There are already five guys who could win any given race, so next year sounds more exciting. Since there is really no point to this article, there is no eloquent way for me to end it, so I will tell you more funny things I said or heard. This is the first time I ever heard the term "super hermaphrodite." I declined my invitation to the mosh pit for fear of meeting the super hermaphrodite.

Mark Hyde, dressed as a small town preacher, accepted Tom Webb's and Guy Cooper's prize packages. I wonder just how much of this stuff they will ever see. One of the hot topics during the riders meeting was should riders be allowed to ride quads the day before racing a bike? It was decided in somewhat loose terms that if you had been riding quads before riding bikes, you could race both, or if you were slow and nobody cared. It was decided riders could still walk the course before hand. I made sure it was okay to pull a wagon with you for your cooler.

Dave Coombs and his family do a wonderful job promoting these races. I can't help but think he doesn't have the NASCAR high bank tri-oval popularity in mind. The Coombs' are well on their way; did you realize they have live radio coverage of the races? ESPN promised more air time, Racer X pledged better coverage for '96, Clipper even said he was going to race the series and take the Senior class away from Tom Webb. Don't miss your chance to watch or race against some of the country's best riders, '96 looks like it is going to be the best year yet. Get off my dead lazy butt and come out and play. The end. □



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Top 20 Overall Bikes

Scott Plessinger	KTM 156
Scott Summers	Hon 156
Fred Andrews	Yam 152
Duane Conner	Yam 135
Thomas Norton	Yam 134
Guy Cooper	Suz 134
Brian Keegan	Yam 102
Tim Shephard	Yam 94
Doug Blackwell	Yam 92
Frank Keegan	Yam 82

Pro AA

Scott Plessinger	KTM 156
Scott Summers	Hon 156
Fred Andrews	Yam 152
Duane Conner	Yam 136
Thomas Norton	Yam 134
Guy Cooper	Suz 134
Doug Blackwell	Yam 117
Robert Bennett	Kaw 101
Thomas Carson	Hon 93

Open A

Richard Bryan	KTM 153
Michael Matoszkia	KTM 147

Charlie Williams	KTM 135
Scott Fontaine	KTM 135
Greg Holden	KTM 125
Thomas Asher	ATK 112

250 A

Brian Keegan	Yam 159
Frank Keegan	Yam 150
Craig Jones	Kaw 141
Daryl Conner	Yam 138
Todd Morain	Yam 134
Ron Palermo	Yam 114
Clint Bardonner	Yam 113
James Rodgers	Yam 95
Troy Goetz	KTM 88
Brian Sovak	Yam 71

200 A

Jim Jarrett	Yam 157
Curtis Bardonner	Yam 144
Joshua McLevy	Hon 136
Heath Bennett	Yam 135
Jason Haines	Kaw 134
Jeff Lauth	Kaw 128
John Bennett	Yam 125
Shawn Belleville	Yam 114
Ronald Johnson	Hon 106
Eric Weiland	Suz 102

4 Stroke A

Barry Hawk	Suz 156
Ralph 'Be' Thorn	Hon 152
Don Bohn	KTM 147
Mark Martin	Hon 132

Vet (30+) A

Tim Shephard	Yam 160
Jeff Murgel	Yam 143
Jeff Russell	Yam 131
Michael Holbert	Kaw 129
Paul Curry	Yam 122
Dan Palermo	Yam 113
Mike McCarren	Yam 107
Mark Smith	KTM 105

Senior (40+) A

Tom Webb	Suz 159
Terry Mealer	Yam 153
Paul Michels	ATK 145
Charles Dailey	Hon 135
Mickey Dorman	KTM 135

Super Senior (50+)

Joe Lojak	Yam 158
David Dehner	Yam 150
Harry Greenlee	Yam 145
Duane Baumgras	KTM 124

Open B

John Grimm	Yam 156
Norman Stout	Kaw 149
Craig Goemmer	KTM 138
Jeff Corra	Hon 129
William Kaufman	KTM 128
Steve Linton	KTM 113
Neal Singleton	KTM 104

Daniel Anthony	Kaw 103
Martin Sprecher	KTM 98
Mark Pahanish	Kaw 156
John Shaffer	Yam 149
Darrius Lattea	Yam 148
Chris Hicks	Kaw 147
Craig Cieslik	Hon 138
Scott Phillips	Yam 113
Craig Lusk	Yam 108
Jim Kelly	Yam 89
Paul Blaner	SUZ 86
Kevin Wright	SUZ 86

200 B

Jeremy Berwanger	Yam 158
Patrick Cumpston	Kaw 143
Joseph Hamor	Yam 137
Kevin Gnapp	Yam 134
Dave Ash	Yam 131
Jason Rankin	Yam 114
Matt Bennett	Kaw 113
William Hindman	Kaw 89
Charles Hager	Hon 87

Youth 100cc (12-15)

Justin Conkle	Kaw 120
Bonnie Hamrick	Yam 71
Josh Spence	Kaw 55

4 Stroke

Steve Stiller	Hon 151
Thomas Evans	Hon 141
Terry Meek	Suz 138
Joseph Lukaesko	Hon 135
Ronald Campbell	Kaw 113
William Bromley	Suz 108
Shane Munger	KTM 106
Marshall Young	Hon 92
Chris Curcio	Hon 89
John Blair	Hon 83

Vet (30+) B

Jay DeRoche	Yam 155
David Molnar	Yam 149
Steve Cooke	Yam 134
Randy Fink	Yam 128
Troy Schlereth	Hon 127
Andrew Wells	Hon 126
Brian Haney	Suz 112
Clark Munger	Suz 108

Senior (40+) B

Michael Schwartz	Suz 157
Richard May	Yam 147
Frank Erbe	Yam 138
Albert Chuppa	Yam 123
Robert Brendle	KTM 122

125/200 C

Terry Spencer	Yam 157
Chad Jarvis	Kaw 151
Jason Smith	Suz 140
Damien Ozoskey	Yam 137
Robert Jarrett	Kaw 113
Craig Jarvis	Kaw 82
Timothy Sheriff	Yam 81

250/Open C

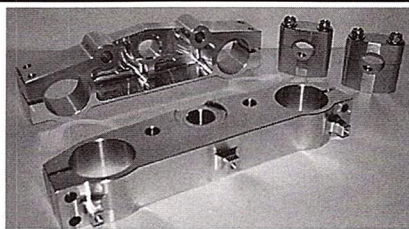
Shawn Rogers	149 Hon
Mark Weiland	132 KTM
Ronald Stetz	125 Yam
Troy Wertz	124 Suz
James Vicheck	118 Yam
Michael Douglas	117 Yam
Mike Denison	103 Suz
Mark Yukevich	87 Yam
John Rush	83 KTM
Brian Channell	77 Yam

Youth 85cc (7-11)

Seth Patton	100 Suz
Ryan Berkfield	91 Hon
Joel Bennett	88 Kaw
Andy Schwaneckamp	76 Kaw

Youth 85cc (12-15)

DR Atwood	67 Kaw
Steven Graves	100 Yam
Charlie Paskins	92 Yam
T J Sullivan	90 Suz
Joshua Knapp	79 Kaw
Jeff Eggleston	69 Suz
Brandon Shriner	67 Yam
Luke Bennett	54 Hon
Brad Hamrick	52 Yam
Willie Westbrook	52 Hon



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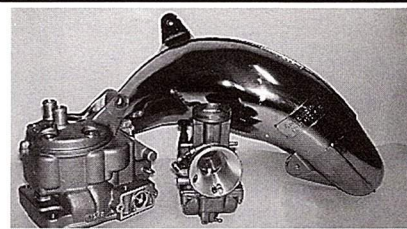
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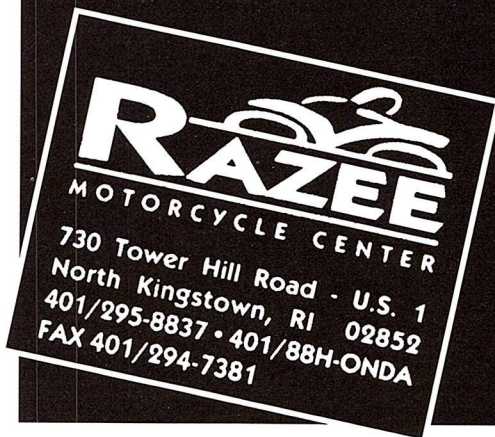
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by Mark Uth

Resolutions

I know Clipper is going to give me a hard time about the column this month, with it being closer to Valentine's Day than New Year's day, but quite frankly, it's hard to get acclimated to magazine time. You see, magazine time, here at Trail Rider, runs about six to eight weeks ahead of normal time for the rest of the East Coast, and is perhaps exponentially different than the geologic clock followed by left coast types. What that means is that any New Year's resolution piece has to be written around the time I'm usually thinking about turkey. Truth is, I'm not much for resolutions of any kind, and thinking about the New Year six weeks ahead of time just doesn't cut it. I'm lucky if I can plan things six days ahead of time; some times that compresses to even six hours into the future. Clearly not in control of events as we know them.

So, while the date at the bottom of this tube reads pretty darn near one January, the printed copy doesn't reach you readers until February. So be it. Anyway, the idea here is that while motorcycling provides near continuous fun, many riders lose interest and/or inspiration, falling into that proverbial rut, no pun intended. Fortunately our position here at TR affords us regular opportunities to break from the norm for something different and/or adventurous, call it a seven month itch or whatever. These changes serve to rejuvenate our premise, and no doubt could apply to any rider approaching the wall, or even lifetime AMA membership status. So without further ado, here's my recommended resolution list, new year's or otherwise, that'll offer a rejuvenated outlook to all for the upcoming season.

1. Go night riding. Every time we suggest this we get a bunch of funny looks and most riders think we're just plain crazy. Truth is, in some instances riding at night affords the best and safest opportunity to really let it all hang out. There are some times when night time dirt riding not only makes sense, but is no doubt preferable. In summer when it's too hot to ride during the day, the night provides cooler temperatures and traffic-free roads and trails. In the unlikely event that another vehicle is encountered, it's seen literally miles ahead, due to headlight illumination. All of a sudden, boring connector roads can become blitzy attack runs. Darkened conditions allow railing of two track and fire roads without fear of oncoming vehicles! The shortened daylight hours

of the winter make night riding a potential necessity if you're going to get any week day riding in at all, and we're strong proponents of week day riding, mind you.

A couple of hints when venturing out after dark. First, choose familiar trails. Fact is that even well known trails take on a completely new look after dark, and simple two track roads can be a blast when freed from the potential of unexpectedly meeting oncoming vehicular traffic. Other does and don'ts: always ride in groups, carry a spare headlight bulb and flashlight, stick to known trails (read: avoid unknown trails and exploring), and finally, beware of four-legged wildlife.

2. Get a buddy hooked on dirt bikes.

The dirt bike bug can be truly amazing, once bitten. I've had some of the most timid of friends turn into complete moto-heads over a season's worth of learning to ride. I'm talking people that wouldn't use a hammer for fear of injury. And no doubt, some of the best times riding can be plunking along with novice riders and showing them the ropes of owning and riding a bike. Lend out that spare scooter and send someone down the path. It'll change their life.

3. Stud up your tires and roost up the snow and ice.

For true New Englanders, this is SOP for winter riding, so no giggling, you guys. However, many riders from the mid-Atlantic, midwest or other more temperate states either quit riding when the ground is snow covered or risk bike and body with unstudded tires. The excuse most often given is that for part time winter riders, a set of real spiker tires like Trelleborgs can seem prohibitively expensive, costing \$200-\$300 per tire. These same meats can be quickly ruined if ridden on plain soil.

What most riders don't realize is that old worn out knobbies studded with screws offer an inexpensive, easy to install alternative that can make you feel like a hero in the snow and ice. Screw options include specific ice screws like Gold Max or Kold Kutters, or even general purpose hardware store variety hex-head machine screws. Preferred sizes are: front: #10 or #12 by 1/2-inch, rear: #12 by 5/8-inch. Surprisingly, regardless of choice, all screw projects end up costing about \$12-\$16 per tire. This assumes one screw per knobby, around 250 screws for a typical rear tire while a tad less than 300 needed up the front. Now I'm sure that seasoned ice riders will jump all over us, mentioning hardware store screws in the same context as Kold Kutters, as no doubt they provide better performance and longevity. And for that matter, be advised that many seasoned winter riders put two and three screws in wide center knobs. However, you can't beat the availability of generic screws and for strict play riding, a single hardware variety screw in each knobby offers plenty of traction and all too much fun for responsible adults (also know in advance that screws may not be legal for competition, even though spiked tires are. If in doubt, ask the promoters.).

4. Participate in some epic international motorcycle event. Work or ride, pit crew or pilot, it doesn't matter, you just haven't

lived motorcycles until you've traded sweat and suds with the international motorcycling scene. Events like the ISDE and Nevada Rally immediately come to mind. Unfortunately, we'll probably see neither of those locally for some time to come. However, you don't have to look far to find some worthy happening. It should be noted that sponsors may often accept volunteers to assist during these events, while paying for their associated room and board for the duration. Failing that, scraping together the money to join the AMA ISDE tour is well worth the cash, believe us. Write letters to sponsoring organizations volunteering countless hours of slave labor, scrape your funds together if you must, stow away on some tramp freighter, just do whatever it takes to become a player in one of these happenings.

5. Write your congressman. This is your good deed for the year. If you have some local issue facing your ability to ride or register dirt bikes, write state representatives in a short, clean, concise method explaining your position as a responsible adult and dirt biker. If you live in dirt bike nirvana and face no pertinent local issues, write your U.S. congressman instead, to support/complain about the lack of funding being allocated for the national recreational trails program, helmet laws, national speed limits or the like. If you're wearing a halo, write 'em both.

6. Take a motorcycle adventure vacation. The list of places to ride away from home seems to grow every year. Baja California, Nevada, Six Days of Michigan, Colorado, Costa Rica, Montana, Guatemala, Alaska, British Columbia; the list goes on. Name just about any destination with open country and you can probably find several outfitters ready to rent you bikes and take you riding. And it doesn't have to be prohibitively expensive either! Many outfitters operate hand to mouth type operations and can be bargained with (to an extent), especially during the off season. Most outfitters are more than willing to customize trips for as few as three riders or as short as two days, in order to accommodate your schedule or pocket book. Call them and ask. Trips can often be combined with business or other travel opportunities to save on air fare or coincide with vacation schedules. Trailering out your own iron is another cost saving ploy, but not recommended—half the fun of such rides is letting someone else worry about keeping the bikes running!

7. Teach a kid how to ride responsibly. This is a bit of self interest, no doubt, but the X Generation are the ones who will shape dirt biking over the next several decades. The notions of bike stealth, tread lightly and other gentleman biker images are worth nurturing for best political impact and continued land access. Additionally, these kids will be at the front of the fight, making sure we have some place to ride, a legal dirt bike registration process and health insurance when we're injured, all at time when we'll probably be putting around on four-stroke dual sport mounts anyway. Consider it a legacy. □

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80/100X21	120/90X18 5 Ply.....	73.20
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FRONT.....

90/90X21.....68.04

REAR.....

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130/80X19.....74.67

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REAR.....

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120/90X18.....78.35

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MS 1110 SOFT

FRONT.....

90/90X21.....56.72

REAR.....

120/90X18.....64.32

130/80X18.....65.85

140/80X18.....71.44| | | |
| --- | --- | --- |
| 120/80X19 | 130/70X19..... | 66.61 |

AP 1110 INTER.

FRONT.....

90/90X21.....56.72

REAR.....

120/90X18.....64.32

130/80X18.....65.85

140/80X18.....71.44

120/80X19.....67.19

ELITE

FRONT.....

90/90X21.....58.43

REAR.....

120/90X18.....67.56

140/80X18.....75.12

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FRONT.....

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REAR.....

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140/90X18.....99.95

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120/80X18.....51.40

130/80X18.....63.14

4.10X14.....36.00

4.10X18.....41.23

4.60X18.....52.94

5.30X18.....61.68

100/90X19.....53.98

110/90X19.....52.83

3.00X21.....34.42

5.30X17.....63.03

3.25X21.....39.23

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2.50X10.....9.08

2.75X3.00X12.....13.17

3.00X14.....18.71

3.50X14.....24.63

2.50X16.....17.88

2.75X17.....18.09

3.00X17.....24.46

3.50X17.....27.98

3.00X18.....26.31

2.75X19.....19.62

3.00X23.....33.43

ENDURO 4

FRONT.....

410X14.....29.15

300X16.....22.94

460X17.....35.00

530X17.....50.94

410X18.....33.37

460X18.....42.82

530X18.....50.31

300X21.....27.77

CITY STATE ZIP

1996 GREENBRIER ENDURO



March 31, 1996 8:00 A.M.

Drawing March 13, 1996

PRE-ENTRIES \$30

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80 MILES OF SOUTH JERSEY'S FINEST TRAILS

Directions:

From the North West: Follow 295 or the NJTP South to 42 East to 55 South to 49 East.

From the North East: Parkway south to Exit 26 and follow arrows.

From Delaware, MD, VA: Delaware Memorial Bridge to 40 East (signs for Atlantic City), to 55 South to 49 East, follow arrows.

Rider Requirements:

All riders must have registration, license plate, motorcycle drivers license, proof of insurance, spark arrestor/muffler, headlight and taillight. **Proof must be shown at sign-up**

The enduro is located in Belleplain, NJ, on Route 550 at the fire house. Belleplain is located about 15 miles east of Sea Isle City, near Woodbine. Please look at a map before you call for directions. Maps are sold in most convenience stores and gas stations. There is plenty of primitive camping available. Please no fires. The route to the start will be arrowed from most major intersections. From the northwest: follow Route 295 or the NJTP to 42 east to 55 south to 49 east and follow arrows. From the northwest: Parkway south to exit 26 and follow arrows. From Delaware, Maryland, Virginia: Delaware Memorial Bridge to 40 east (signs for Atlantic City) to 55 south to 49 east. Follow arrows.

There will be one gas stop. A truck will be provided for the gas stop only, not the gas available. Only red cans will be allowed on the gas truck. All refueling must be done in the refueling area. Please obey the fire marshall.

The landowners who have given permission for our enduro ask that you do not come down to ride in their woods afterwards.

Some of our turn mileages are up now. Anyone caught trespassing will be beaten, prosecuted and disqualified!

Two dollars of your entry fee will be used to support the New Jersey Trails Conservancy.

Per ECEA rules, no trophies or points will be awarded to non-finishers. Any trophies not picked up at the event will be taken to the next ECEA meeting. If no one from your club takes it, it's kindling.

Send entries to:

Tri-County Sportsmen M.C. • P.O. Box 146 • Port Elizabeth, NJ 08348

For information call: (609)785-2754 • Please limit calls!

Release and Waiver of Liability and Indemnity Agreement

Name _____ Age _____

Address _____

City _____ State _____

Zip _____

Phone (area code) _____

ECEA No. _____

AMA No. _____ Expires: _____

AMA Club Name: _____

Bike Make: _____ Displacement: _____

Check skill level and class below:

- | | | | |
|--|---|----------------------------|----------------------------|
| <input type="checkbox"/> AA | <input type="checkbox"/> A | <input type="checkbox"/> B | <input type="checkbox"/> C |
| <input type="checkbox"/> Senior (40+) A-B | <input type="checkbox"/> Super Senior (50+) A-B | | |
| <input type="checkbox"/> Four Stroke A-B-C | <input type="checkbox"/> Women | | |
| <input type="checkbox"/> Veteran (30+) A-B-C | <input type="checkbox"/> Masters (60+) | | |

PLEASE READ AND SIGN THIS RELEASE!

I hereby give up all rights to sue or make any claim whatsoever against the American Motorcyclist Association and its district organizations, the sponsoring club of this event, the promoters, sponsors and all other persons, participants or organizations conducting or connected with this event, for any injury to property or person I may suffer, including crippling injury or death, whether such injury arises while I am preparing for or participating in the event, or while I am on the event premises.

I know the risks of danger to myself and my property while participating in the event and while upon the event premises and, relying on my own judgment and ability, assume all such risks of loss and hereby agree to reimburse all costs to those persons and organizations connected with this event for damages incurred as a result of my negligence.

Signature _____

Vehicle No. _____

Witness _____

Address _____

**Participant under 18 must have
notarized parent/guardian signature!**

Parent/Guardian Signature (sign in ink)

Notary _____

Commission Expires _____

ARE YOU FIT ENOUGH TO WIN IN '96?

Five ways to find out just how far you let yourself go over the winter.

by Eddy Henczel

Did you spend the off season training like an animal or did the winter layoff put you in the worst shape of your life? Will the competition shake with fear when you get to the starting line, or is it your beer belly that does the shaking? Or are you like the rest of us and fit in somewhere in the middle? To find out exactly how you stack up against your competition, and to gauge your fitness throughout the racing season, Trail Rider has come up with five different ways to determine your fitness level. Take the tests, chart the results and find out what you need to work on. Just do it before the competition does.

Heart Rate

If you have enough strength to kick-over your bike and twist the throttle, then the next most important physical requirement a racer has is cardiovascular endurance—the ability of the heart, lungs and blood to take oxygen from the atmosphere and distribute it throughout the body. A fit heart pumps more blood with each beat than an unfit one, and therefore needs fewer beats to deliver oxygen to the cells. This is especial-



You may have won your class, but if you don't have the strength to open a Power Bar, you're in trouble.

ly important at the end of a long race. People who are fit tend to have lower resting heart rates than unfit people. A person is considered unfit if their heart beats more than 80 times per minute while resting. An average person's heart beats between 70 and 80 times a minute, a fit person's resting

pulse is between 50 and 70, and a very fit athlete has a heart rate of 30 to 50. The lowest recorded resting heart rate (from an extremely healthy cross-country skier) is 27.

The Cooper Test

In the '60s, Dr. Kenneth Cooper encouraged readers to exercise until exhaustion. Although he has since softened his stance on extreme physical exertion, The Cooper Fitness Test is still a good judge of physical conditioning today as it was for the Hodaka generation. The premises of the test are simple—run 1.5 miles (six laps of a standard quarter-mile track) as fast as you can and then compare your time to Cooper's chart. (Women add 2:45 to the times.)

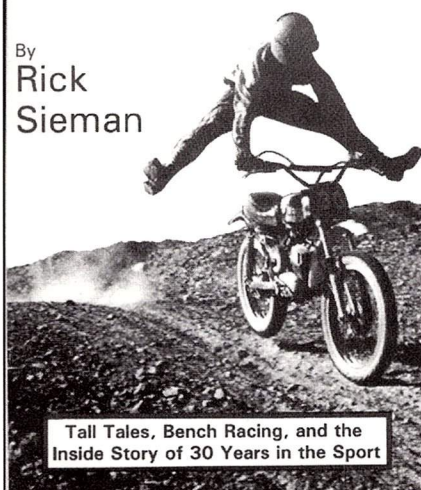
The U.S. Army

To graduate from boot camp grunts must run three miles in under 21 minutes. While being able to crank out seven-minute miles is a good judge of a recruit's aerobic fitness, the Army demands more than just the ability to run. The most demanding outfit in the Army is the Ranger's Special Operations section. Just to enter the Rangers training program recruits need to run two miles in less than 14:54, perform 52 push-ups in two minutes, 62 sit-ups in two minutes and

620 US/1226 Canada

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By
Rick
Sieman



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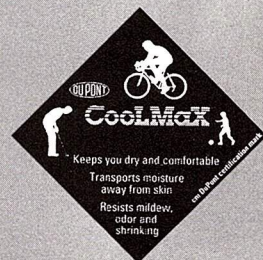


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Age Fitness Level:	Cooper Test Chart			
	20-29 yrs	30-39yrs	40-49yrs	50-59yrs
Very poor	>16:01 min.	>16:31	>17:31	>19:01
Poor	14:01-16:00	14:44-16:30	15:36-17:30	17:01-19:00
Fair	12:01-14:00	12:31-14:45	13:01-15:35	14:31-17:00
Good	10:46-12:00	11:01-12:30	11:31-13:00	12:31-14:30
Excellent	9:46-10:45	10:00-11:00	10:30-11:30	11:00-12:30
Superior	<9:45	<10:00	<10:30	<11:00

be able to do six pull-ups. Being able to field strip a M-16 in under ten minutes is good way to earn bonus points, but knowing how to make the forks on a CR250 work will get you promoted to the motor pool.

President's Physical Fitness Awards

Remember high school? Remember the President's Physical Fitness Awards? Remember how badly you wanted a nice laminated scroll signed by the President so you could hang it on your bedroom wall? Remember how you failed to meet the physical requirements and ended up decorating your room with Roger DeCoster posters instead? If you think you're in better condition than you were in your senior year, try the test again. Men must run a mile in under 6:07, perform 55 sit-ups in one minute and do 13 pull-ups in one minute. Unfortunately you have to be under 19 to get a certificate from President Clinton, but if you meet the requirements, you'll be happy to know you're in better shape than most high school students and 125cc beginners.

University of Oregon

According to University of Oregon professor Gary Klug's book, Exercise and Physical Fitness, there are four key indicators to a person's fitness level: flexibility, strength, cardiovascular fitness and body fat. Flexibility refers to the elasticity and smooth, easy movement of the joints and muscles. To a racer, back, hip and hamstring muscles are the most important. To test for flexibility of these groups, Klug suggests the sit-and-reach test. Sit on the floor with your feet in front of you, about 12 inches apart. Tape a yardstick to the floor between your legs lengthwise - the 15-inch mark should be at your heel and the one-inch mark should be near your knees. Keeping legs straight, lean forward and touch as far down the yardstick as possibly. After three attempts, record your best result.

Excellent . . . 24-27 inches
Good21-23 inches
Average . . .17-20 inches
Fair13-16 inches
Poor0-12 inches

Strength refers to the amount of force that can be exerted by a single muscle contraction. For this test, Klug uses the sit-up test. With knees bent, hands behind head, do as many sit-ups as you can in a one-minute period.

Excellent . . . 46 or more
Good35-45
Average . . .21-34
Fair10-20
Poorless than nine

By using a simple at-home test, a person can easily measure their cardiovascular recovery rate. All you need is a bench or a steep step (a bike stand is too high). To begin simply step onto the bench with your left foot and then follow with your right foot. Step off using your left foot first. Continue at a moderate pace for three minutes, wait one minute and take your pulse for 30 seconds. Wait another minute and take your pulse for another 30 seconds. Take your

pulse again after waiting another minute. Add the three times together to get your recovery rate.

Excellent . . 140 or less
Good141-157
Average . . .158-175
Fair176-192
Poor193 or more

Everyone needs some body fat, but most of us pack around too much. The percentage of fat in healthy humans can vary greatly, anywhere from five to 40 per cent. By nature, women have more fat than men, but anyone over 40 per cent is considered obese. A person can check their body fat composition a number of ways - a caliper skin-fold test or an electrostatic weighing tank being the most accurate. However, unless you have the proper equipment and expertise, these are too complicated. The National Institute of

Fitness and Sport recommends standing, pinching the fold of fat around your waist and measuring the depth of the fold.

Very low fat less than 1/4 inch
Low fat 1/4 to 1/2 inch
Average 1/2 to 3/4 inch
Above normal 3/4 to 1 inch
Very high over one inch

And the survey says. . .

Although results may vary slightly from tests to test, they are as accurate as you can get using little or no equipment. For a more comprehensive test, you could always drive down to the Cooper Aerobics Center in Dallas, fork over \$300 and take their Ultra Fit Test. On the other hand you could put the money towards gas, drive south until you find a riding area that isn't covered in snow and ride until you're as skinny as Doug Henry. Any leftover change can be put towards a six-pack of diet beer. □

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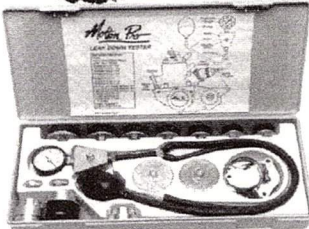
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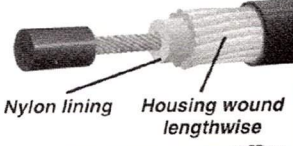


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1992 Kawasaki KX250 Two for sale, both exc. cond. Need nothing, moto and enduro trim. \$2000 each, or \$3800 for the pair. (914)424-4904.

Do you have a 1981 or '82 KTM 250 Enduro twin-shock in the back of your barn or garage? I want one, in nearly any condition, although it must be cheap. Call Paul at (609)953-7805.

1995 Honda CR125 Pro-Pilot susp., Pro-Circuit pipe, Boyesen reeds, tall non-slip Ceet seat, Renthal bars and sprocket, all fresh, exc. cond. Visa & M.C. accepted, \$2975. (908)493-0356.

1995 Honda CR250 Pro-Pilot susp. and motor, FMF pipe, Boyesen reeds, tall non-slip Ceet seat, Renthal bars, steel clutch, o-ring, all fresh, exc. cond., Visa & M.C. accepted, \$3575. (908)493-0356.

Scott Plastic MX Boots Large shell, size 10 inner boot. Yellow/black, exc. cond., \$100. (908)493-0356.

1991 Suzuki RM250 Immaculate cond., Factory Connection susp., new sprockets, chain. Meticulously maint'd, Vet owned, must sell. \$1500 obo, (802)888-2922.

1992 Husqvarna 250WXC Factory Connection susp., Dyno Port torque pipe, Acerbis 035 silencer, Technosel seat, MXA graphics, W.O.R. wide pegs, hare scrambles/enduro setups (number plate, headlight taillight & extras). \$1800, 1-800-839-5825 beeper any time; (516)667-2790 days.

1992 ATK 406XC Exc. cond., lights, odo, new tires. \$1800, (516)283-0898.

Sinsalo Riding Pants Like new, size 32-33. blue and white, \$75. Will throw in free MS tool pouch and Sinisalo kidney belt. Call Anthony any time, Queensbury, NY (518)745-5871.

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1995 Husqvarna WXE 350 Exc. cond., Enduro Expert susp., many extras—brakes, sprockets, etc. \$4500 obo, call Darrin, (703)817-7740.

1996 KTM 300MXC Bought new Sept. 31, must sell. \$3995. (540)858-2634.

1996 Honda XR400R Brand new, never ridden. Thumper Racing exh kit and jets, still in box. \$4500 obo, (603)430-8581.

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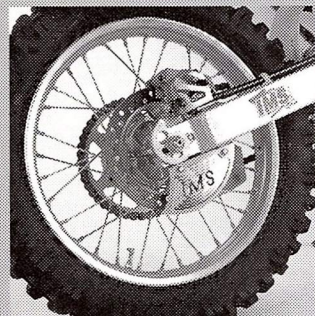
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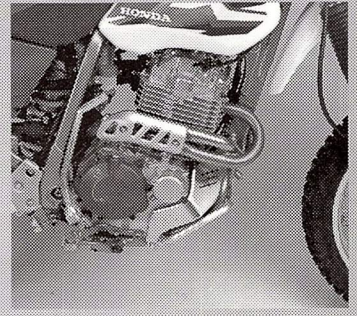
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ANY PLACE	9:32	40.8 24
ANY ENDURO	9:33	41.2 24
ANY TRAIL RIDE	9:34	41.6 24
ANY PLACE	9:35	42.0 24
ANY ENDURO	9:36	42.4 24
ANY TRAIL RIDE	9:37	42.8 24
ANY PLACE	9:38	43.2 24
ANY ENDURO	9:39	43.6 24

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Meteor MC Fall Hare Scrambles

New life for the Chatsworth Triangle

by Mark Uth, photos by Jungle Dave

Chatsworth, NJ 11/19/95

The infamous "Triangle" riding area, located near Chatsworth, New Jersey, has long been a winter riding spot used by snow-bound riders from the north and west. If you happened to have paid a visit there lately, you'd have found several not-so-subtle changes afoot. Of immediate concern were the newly posted "No Trespassing" signs littering all borders and access roads. Those who ignored the signs and pressed on might also have found the pit area to be freshly cleaned and completely devoid of burned up car hulks and/or major appliances. This would normally elicit an "Oh no—another yuppie paradise development starting in the low 180s" and accompanying intestinal distress. Fortunately, however, in this case it's actually the motorcycling public, namely the New Jersey Trails Conservancy, taking charge and running off the riffraff (e.g. illegal dumpers, car-burners, helmetless pit squids), while carving out perhaps a permanent recreation area for NJ knobby tire enthusiasts.

November marked the first event run in the newly reclaimed area, an ECEA hare scrambles hosted by Meteor Motorcycle Club. Held on a gloomy day with calm winds and seasonal temperatures—the daytime high might have hit 50 degrees—great for riding but a bit chilly for spectating. In spite of that, turnout for the event was extraordinary as more than 130 "bikes" raced that day, and literally hundreds of spectators lined the course. We use the word "bikes" loosely here, as a separate heat for fellow NJTC members riding ATVs was included in the day's competition. On top of all that, numerous local dealers were represented in the pit area hawking parts and accessories for any impulse buyers in the field.

The course was laid out by trail boss Mike McHale, leading many to speculate about its potential difficulty. The terrain within the triangle includes the characteristic South Jersey pine and deciduous forest found around Chatsworth, a few water obstacles within the pits, complicated with patches of wet clay-based soil. It turned out, however, that there were few really tight or technical sections. Instead, riders were greeted with plenty of pit action and well worn trail. Quads ran a one hour moto over a five-plus mile course first thing in the AM. Running the quads first served to widen the trail somewhat (and create tricky two-track ruts) as bikes ran nearly 100% of the quad course plus a couple of extra doglegs here and there, for a loop totaling up to a tad over 6 miles.

After completion of the ATV race, a two hour bike-only scramble was planned using the "Delaware type" continuous scoring system. The start was conducted in three waves by class, and it was quite the crowd

pleaser, as the course wound among many deep puddles (correction: small lakes) found within the pit. Many competitors



A rock section in South Jersey? How'd they do that? Lee Rothstein gets the holeshot, while Mike Lafferty douses Joel Dengler on the start.

turned out that most of the go-fast guys completed 12 laps, which roughly equates to a whopping 36 MPH speed average.

After the Lafferty DNF, Philip Carlin assumed the second place slot and held on for second overall and first place in the A Heavy (250cc+) class. Frank Vanaman rounded out the podium with a third overall finish. In the lower classes, Steve Larkin was the premier B class finisher while Chris Rindone earned top honors within the Novice class. Scott Wolfersberger topped a combined (A&B) Senior class and (no snickers, please) Frank Anastasio piloted his Honda quad to the ATV class victory.

Trophies and prizes were distributed to top finishers and as things wound down in the late afternoon, some pertinent observations began to coalesce. Foremost is the two-pronged coup pulled off by the NJTC, providing a respectable, well-groomed face for the NJ off-road public and subsequently convincing cognizant authorities that a piece of ground like the Triangle could best be managed by the NJTC. Hats off to the group, and to the ECEA clubs actively supporting the crusade. It should be noted that

with the advent of the NJTC control, the property was posted to restrict access to card-carrying (well, actually, permission slip carrying) NJTC members. It's fitting that Meteor, one of the more pro-active NJ clubs, reaped first benefits of our new found clout, putting on a fine showing and well received event. Attaboys all around. Similarly, look for a PBER hare scramble to be held in the same area sometime near the onset of this new season, and with this fall's popularity as a barometer, PBER should turn out the masses in force. □

ving for the ultimate direct line were found cutting through the edges of the puddles and finding out how surprisingly deep they are. This resulted in a total soaking of themselves and anyone unfortunate enough to be within roosting distance.

It didn't take long for pre-race favorites to assert themselves. Mike Lafferty, recently returned from his rookie season on the national enduro circuit, assumed the early lead aboard his KTM 250. Pro MXer Joel Dengler trailed aboard a trick lime green RM125, stuck to his fender like a Siamese twin. Lafferty and Dengler continued in this moto dance for the first four laps, setting a blistering pace and running away from the rest of the field. Eventually however, bad luck befell Lafferty as a fried clutch soon led to an untimely DNF. Dengler continued to cruise for the full two hours, completing 12 laps and earning the overall victory. It

Meteor Hare Scrambles

Class Results

Grand Champion

Joel Dengler (A - 125cc)

Suz

A - 125cc - 249cc

- | | |
|--------------------|-----|
| 1. Dale Hiles, Jr. | Yam |
| 2. Sean Tompkins | Suz |
| 3. Mark Lachowicz | Hon |

A - 250cc and Up

- | | |
|-----------------------|-----|
| 1. Philip Carlin | Hon |
| 2. Frank Vanaman, Jr. | KTM |
| 3. Bill Atkinson | Hon |
| 4. Terry Tucker | Kaw |
| 5. Lee Rostien | Yam |

B - 125cc - 249cc

- | | |
|----------------------|-----|
| 1. Ronald Lucas | Kaw |
| 2. Scott Taylor | Kaw |
| 3. Howard Lichtman | Kaw |
| 4. Kevin Kamuca | Kaw |
| 5. John Petrine, Jr. | Hon |

B - 250cc and Up

- | | |
|------------------|-----|
| 1. Steven Larkin | Hon |
|------------------|-----|

- | | |
|--------------------|-----|
| 2. Rick Stapleford | KTM |
| 3. Al Switzer | KTM |
| 4. Mark Perry | Yam |
| 5. Douglas Abranow | Hon |

C Class

- | | |
|--------------------|-----|
| 1. Chris Rindone | Suz |
| 2. Thomas Britton | Hon |
| 3. Rob Comber | Kaw |
| 4. Michael Wible | KTM |
| 5. Steve Augustine | KTM |

Senior - 40 Yrs and Over

- | | |
|------------------------|-----|
| 1. Scott Wolfersberger | Yam |
|------------------------|-----|

- | | |
|-----------------------|-----|
| 2. Tom Ebersole | Hon |
| 3. Thomas Smith | Hon |
| 4. Jack Lafferty, Sr. | KTM |

- | | |
|--------------------|-----|
| 5. George Clickner | Hus |
|--------------------|-----|

ATV - All Sizes

- | | |
|-----------------------|-----|
| 1. Frank Anastasio | Hon |
| 2. Thomas Meeks | Suz |
| 3. Christopher Butler | Hon |
| 4. Mario DePalma | Hon |
| 5. David Meeks | Suz |



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The Prodigy

I sat on the curb in front of my house with half a headache from the deafening sound of birds waking, and I watched a man leaving the widow's place trying to act nonchalant which is a difficult act to play when your shirt's buttoned wrong, you're tripping over untied shoelaces and you can't remember where you parked your car. The widow was running true to form; first she marries a guy who medium-raises himself hooking up a 220 volt clothes dryer, and now she's aiming at this klutz who can't find his own automobile.

She could start her own chapter of Accident Prone Anonymous.

The clock on the Bultaco read 6:45:00 when Jim's van swung around the corner and it read 6:45:30 when he stopped alongside me—Jim had "zeroed" the pickup time. This is very common among enduro riders who tend to get a little bit nuts about time and mileage. They do this compulsively. If a good enduro rider tells you he's going to call at eight o'clock, you can amaze people by saying, "excuse me, I have to get the phone," BEFORE it rings.

Couple years back the East Coast Enduro Association held their awards dinner at a restaurant which had erected a large sign on the highway stating that the eatery was two miles farther on. It wasn't; it was 1.9 miles. I believe everyone there found themselves passing the place and on the two lane bridge into Tom's River before they took their eyes off the car speedo. That particular restaurant couldn't cook any better than they could read mileage. Kiffy Madkiff, at our table, took one look at the undercooked beef and declared, "I seen steers hurt worse than this and get well!"

Jimmy hopped out of the van and came back to help me grunt my bike onto the trailer. Jim disdains ramps claiming if you can't lift the motorcycle onto the trailer you shouldn't be riding the thing. I was pleased to see he looked a good deal happier than the last I saw him, two weeks ago. At that time he had insisted, over and over, that I ride his new Zundapp. I didn't want to because I have a hard time trying to short-circuit my brain into shifting and braking with the wrong feet. Predictably, I managed to tanglefoot his dumb Zundapp into a half dry mud hole garnished with a peeled log at a 45 degree angle. I mean to say they recorded that one on a seismograph over at Princeton. I folded the right footpeg so far that it was jammed against the case.

After ten minutes or so Jim became concerned over all the silence and rode my Bul out to find his 300 mile motorcycle lying on it's side in a mud hole and me trying to persuade its footpeg back down with a four foot length of lightning-struck pine and blows with the skull of a recently deceased deer. There I was trying to FIX the damn thing and Jim tightened his face about two sizes and said, "don't bother, they don't fold." I felt in the mud and, sure enough,

there was no hinge.

Ever since then, when I see the statement 'non-folding footpegs', I sort of chuckle.

The secret to loading motorcycles early Sunday morning without antagonizing neighbors is this: don't talk, don't even whisper, and above all, don't cough. You can drop your tie-down buckles a hundred times and no one will notice, but I've found that adults will instinctively wake at the slightest sound of men TRYING to be quiet. They just might retaliate by running noisy mowers past your bedroom windows the following Saturday. And they might do it even if you mowed it yourself the day previous.

Before we drove away Jim motioned me to silence with a finger over his lips and pointed to the back of his van where a boy of eleven or so slept in a nest of Barbour jackets, leather pants and oily rags that had been dipped in sawdust. I closed the van door as quietly as possible and Jim rolled down the hill so he wouldn't have to use the starter motor; a unit noisy enough to set car stealing back ten years. The sleeping boy explained the 125cc Lobito on the trailer. I had thought it was just another resurrected basket case Jim is always fooling around with. The boy was Jim's nephew; his parents had gone away on a second honeymoon which is usually a waste of time, as women and old dogs share an aversion to new tricks.

The dogs, however, get fewer headaches.

We went to an unsanctioned cross-country run solely because it was laid out by Romaine Howey, who has his act together and arrows fine ridable courses that usually cloverleaf to the start line a few times. Unsanctioned meant the kid could ride, as the only age requirement was that you be potty-trained and on solid food. The cloverleaf layout meant that bums like me could have more opportunities to quit.

In due time we arrived at a field off Route 9, midway between BRIDGE FREEZES BEFORE ROAD SURFACE and CLEAN FILL WANTED, signed up at the Buick Roadmonster, got our riding numbers and helped the kid start the Lobito. Jim strapped a helmet on him and the little bugger shot over to where some 'shoes' were playing around in a potato field and passed them all, feet up and sliding, then came back to have his nose wiped. The kid was terrific; and if there is one thing I can't stand, it's talent.

Three quarters of the way through the sixty mile event I stopped for the usual items (fuel, chain lube and a fresh Ace bandage for my knee) and saw that Jim was doing real well as he only took time for two bites

from the fried egg sandwich I had hidden up in the springs of the front seat. Jim must have been part beagle and part garbage can to find that sandwich. I had hidden it there three weeks ago and forgotten about it. It wasn't what you'd call the best fried egg sandwich I ever ate.

And it wasn't the worst either.

Jim finished fourth, out of the brass; I was six minutes up on the sweep crew. The kid had quit at the three quarter point and was sleeping in a puddle of mud on the van seat. His Lobito lay on the ground with wheels wrapped solid with sedge grass and yards of vines trailing off the foot pegs. Jim was unhappy missing a trophy but there, on the rear lid of the Roadmonster, sat a lone trophy engraved: THIRD PLACE 125 CUBIC CENTIMETRES. Unawarded, since only two 125cc had finished. Jim could TASTE that trophy. He roused the nephew up, wiped the sleepers from his eyes, made him drink a cup of coffee and sent him down the trail again with a promise of a chocolate nut sundae if he finished and a kicked ass if he didn't. Jim, you see, knew that there were no time limits at a cross country event.

Next, Jim eased himself into the Buick when no one was looking and locked the doors. The starving promoters argued with him past the partly opened vent window that the sweep crew had swept the course but Jim wasn't buying any of it; he KNEW he had a rider out there yet.

A little later they offered to give him the trophy but Jim wanted it PRESENTED, with all due ceremony, to his rider.

It was 9:45 PM when the bobbing headlight of the Lobito came down the trail to the cheers of the promoters, who were partly mad with hunger and stomping their feet to keep warm. The kid looked to be sixty-five years old.

All the way home Jim was touting that kid to be the next National Enduro Champ. But he was wrong, dead wrong. The kid hasn't sat on a motorcycle since 9:46 that night; and still gets nauseous at the sight of a chocolate nut sundae. □

— Ed Hertfelder



Ed Hertfelder is a teller of tales and writer of books, as well as author of the internationally famous Duct Tapes stories. He can be reached at 111 North Summit Dr, RR5, Cabot AR 72023.

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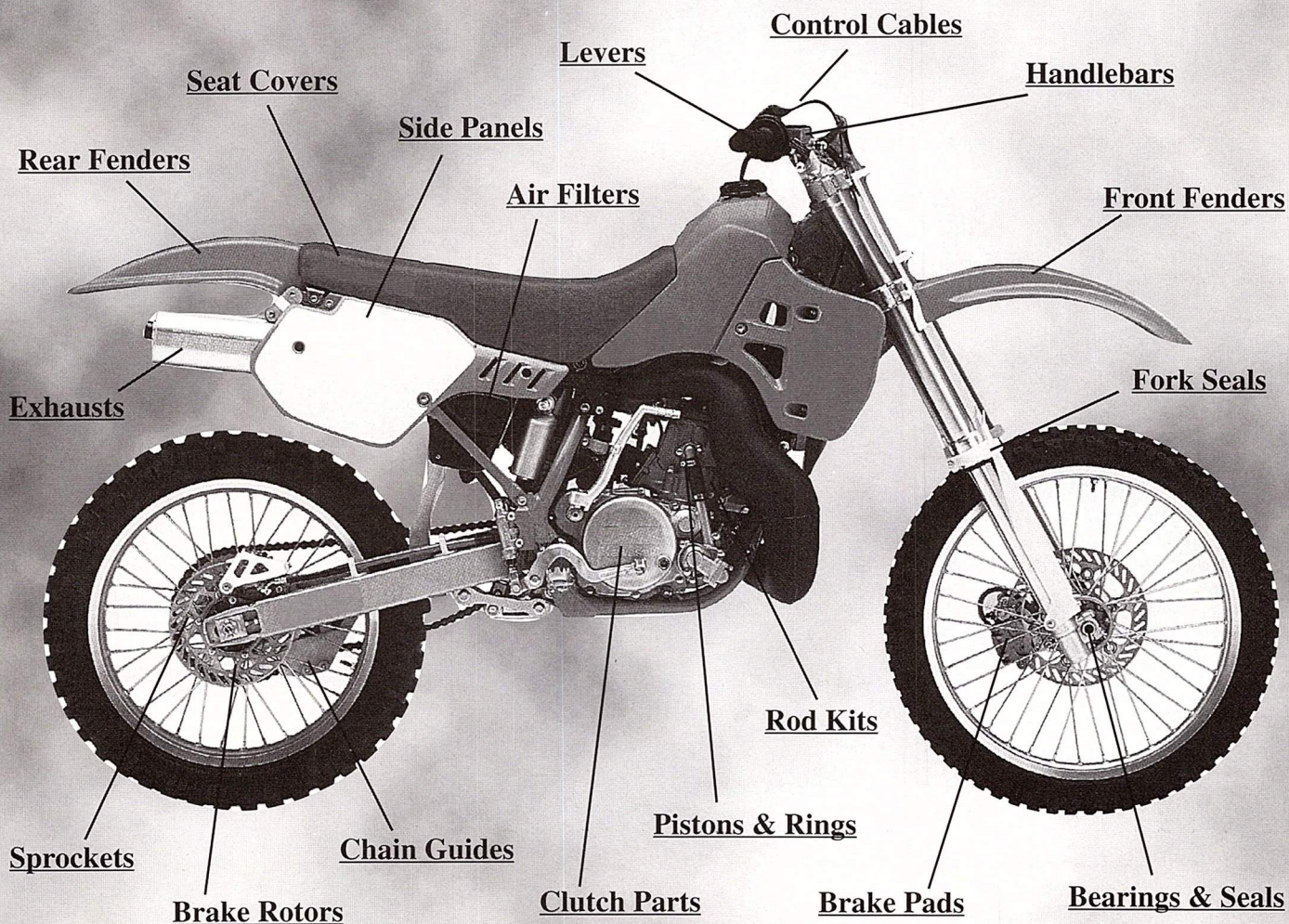
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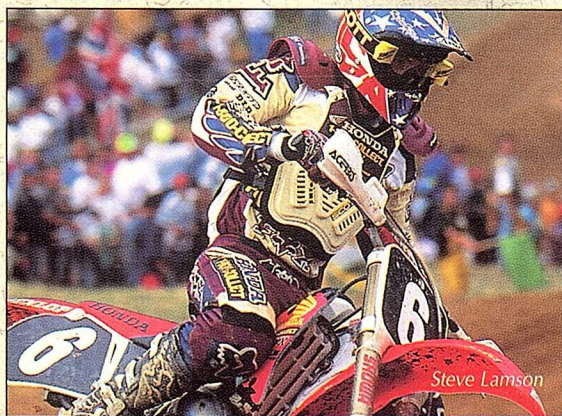
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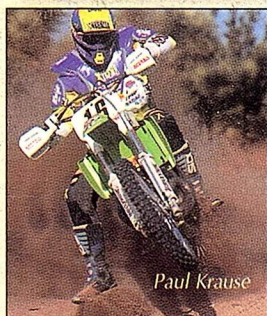
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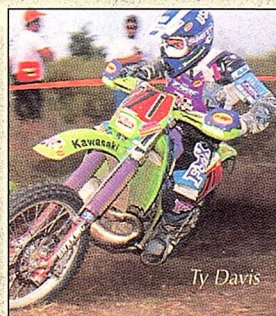


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